



BRIDGE NO.04671

33900 - GRISWOLD
CARROLL ROAD
over
PACHAUG RIVER

Routine and Underwater Inspection

4/14/2021

Inspected by: CONSOR



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Report Title Page

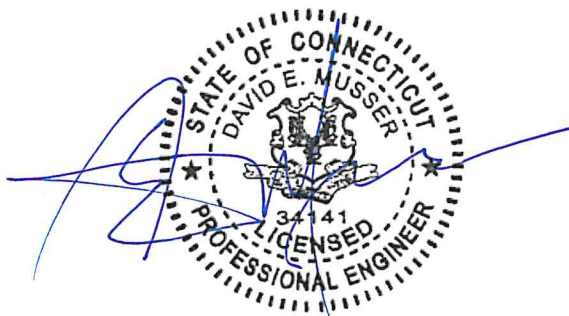
Project No.: 170-3543 (Underwater Inspections – Non-NHS)

Structure: 04671, Carroll Road over Pachaug River, Griswold

Inspected By: CONSOR Engineers **Inspection Date:** 4/14/2021

Professional Certification:

I hereby certify that this report, including all of its contents, has been approved by me, and that I am a duly licensed professional engineer under the laws of the State of Connecticut.



David E. Musser, PE (CONSOR Engineers)

PM / QAQC: David E. Musser, PE

CT License No.: 34141

Date: 4/30/2021

Form: Location

Inspection type: Routine, Underwater

Inspection Date: 4/14/2021

Inspected by: CONSOR Engineers

:Bridge No 04671

Town: GRISWOLD

Carried: CARROLL ROAD

Crossed: PACHAUG RIVER

Inventory Route: Non-NHS



Location Map # 1
Bridge No. 04671
Carroll Road over Pachaug River
Griswold, CT

Form: BRI-19, Rev. 2/15
Inspection type: Routine, Underwater
Inspection Date: 4/14/2021
Inspected by: CONSOR Engineers

:Bridge No 04671

Town: GRISWOLD
Carried: CARROLL ROAD
Crossed: PACHAUG RIVER
Inventory Route: Non-NHS

STRUCTURE INVENTORY & APPRAISAL

INSPECTION

Structurally Deficient Functionally Obsolete
Sufficiency Rating
(90) Inspection Date (91) Frequency
Indepth Insp Proposed next Indepth Year
Deck Survey Date Class
Access Flagman

	Frequency	Date	Type
Fracture	<input type="text"/>	<input type="text"/>	<input type="text"/>
Underwater	<input type="text" value="12"/>	<input type="text" value="04/14/2021"/>	<input type="text" value="A Full Inspection"/>
Special	<input type="text"/>	<input type="text"/>	<input type="text"/>

IDENTIFICATION

Bridge Name
Town Code - Name
(5) Inventory Route
(A) Record Type
(B) Signing Prefix
(C) Level of Service
(D) Route Number.
(E) Dir Suffix
(6A) Featured Intersected
(6B) Critical Facility Indicator
(7) Facility Carried
(9) Location
(11) Mile Post Miles
(16) Latitude Deg. Min. Sec.
(17) Longitude Deg. Min. Sec.
(98) Border Bridge
(A) State Code (B) Percent Responsibility %
(C) Border Town Name
(99) Border Bridge Structure No.

STRUCTURE TYPE & MATERIALS

(43) Structure Type, Main
A) Material
B) Design Type
(44) Structure Type, Approach
A) Material
B) Design Type

(45) Number of Spans, Main Unit
(46) Number of Approach Spans
(107) Deck Structure Type
(108) Wearing Surface/Protection Systems
A) Type of Wearing Surface
B) Type of Membrane
C) Type of Deck Protection

Substructure
A) Material
B) Design Type

Paint
Type
Year
Comment

GEOMETRIC DATA

(48) Length of Maximum Span ft.
(49) Structure Length ft.
(50) Curb or Sidewalk Widths
A) Left ft. in. B) Right ft. in.
(51) Bridge Roadway Width Curb to Curb ft. in.
(52) Deck Width, Out to Out ft. in.
(32) Approach Roadway Width ft.

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(33) Bridge Median
Deck Area sq. ft.
(34) Skew Angle deg.
(35) Structure Flared
(10) Inv. Rte. Min. Vert. Clearance ft. in.
(47) Inv. Rte. Total Horiz. Clr. ft. in.
Log Inv. Rte. Total Horiz. Clr. ft. in.
RLog Inv. Rte. Total Horiz. Clr. ft. in.
(53) Min. Vert. Clearance Over Bridge ft. in.
(54) Log-Min. Vert. Underclearance ref. ft. in.
(55) Min. Lat Underclearance on Right ref. ft. in.
(56) Min. Lat Underclearance on Left ft. in.

CONDITION

(58) Deck
(59) Superstructure
(60) Substructure
(61) Channel & Channel Protections
(62) Culverts
(36) Traffic Safety Features
A) Bridge Railings
B) Transitions
C) Approach Guardrail
D) Approach Guardrail Ends

WATERWAY

Drainage Basin Waterway
(38) Navigation Control
(39) Navigation Vertical Clearance ft.
(40) Navigation Horiz. Clr. ft.
(111) Pier/Abutment Navigation
(116) Vert-Lift Brg Nav Min ft. in.

AGE AND SERVICE

Year Built (106) Year Reconstructed
(42) Type of Service
A) On
B) Under
(28) Number of Lanes
A) On B) Under
(29) Average Daily Traffic
Is Above Half ADT?
(109) Percent Truck %
(30) Years of ADT
(19) Bypass, Detour Length Miles

APPRAISALS

(67) Structural Evaluation
(68) Deck Geometry
(69) Underclearances, Vert. & Horiz.
(71) Waterway Adequacy
(72) Approach Roadway Alignment
(113) Scour Critical

COMMENTS

CLASSIFICATION

(112) NBIS Bridge Length
(104) Highway System
(26) Functional Class
(100) Defense Highway
(101) Parallel Structure
(102) Direction of Traffic

Form: BRI-19, Rev. 2/15
Inspection type: Routine, Underwater
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Inventory Route: Non-NHS

(103) Temporary Structure
 (110) Designated National Network
 (20) Toll
 (21) Maintain
 (22) Owner
 Report Class
 (37) Historical Significance

POSTED SIGNS

Other Posted Sign 1
 Other Posted Sign 2

	Actual	Recomended	
Posted Load Single Unit Truck	<input type="text"/>	<input type="text"/>	tons
Posted Load Semi-Trailer Truck	<input type="text"/>	<input type="text"/>	tons
Posted Load 4 Axle Truck	<input type="text"/>	<input type="text"/>	tons
Posted Load 3S2 Truck	<input type="text"/>	<input type="text"/>	tons
All Vehicles	<input type="text" value="15"/>	<input type="text" value="15"/>	tons

Posted Vert. Clearance on Bridge ft. in.
 Posted Vert. Underclearance ft. in.
 Posted Speed Limit on Bridge m.p.h.

OTHER FEATURES

Fence Required
 Fence Present
 Fence Type
 Fence Height
 Fence Material
 Fence Top Type
 Barrel Ladders
 Stand Pipes
 Catwalks
 Moveable Inspection System
 Haunches Present over Roadway
 Utilities

PROPOSED IMPROVEMENTS

(75A) Type of Work Proposed
 (75B) Work Done By
 (76) Length of Structure Improvement ft.
 (94) Bridge Improvement Cost \$
 (95) Roadway Improvement Cost \$
 (96) Total Project Cost \$
 (97) Year of Improvement Estimate
 (114) Future ADT
 (115) Year of Future ADT
 DOT Bridge Program List No
 Project No
 Advertised Date

LOAD RATING & POSTING

(31) Design Load
 (63) Operating Rating Type
 (64) Operating Rating
 (65) Inventory Rating Type
 (66) Inventory Rating
 Evaluation Code
 Year of Evaluation
 (70) Bridge Posting
 (41) Structure Status

Form: BRI-19, Rev. 2/15
Inspection type: Routine, Underwater
Inspection Date: 4/14/2021
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:Bridge No 04671

Town: GRISWOLD
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Inventory Route: Non-NHS

INSPECTOR'S SIGNATURES:

1)  Date: 04/27/2021


2)  Date: 05/03/2021

3) _____ Date:

4) _____ Date:

P.E. SIGNATURE:  Date: 05/05/2021

P.E. # _____

Reviewed By:  Date: 05/11/2021
William Freeman, Jr.

FIELD INSPECTION REPORT

Location:	100' EAST OF SHELDON
Main Material:	3 - Steel
Main Design:	01 - Slab

Year Built:	1938
Year Rebuilt:	

Snooper Required:
 Snooper Used:

Inspectors:

Lead Inspector:	Stephen Sirotko
Inspector:	Task:
Balboni, Michael	BSE - Inspector
Metsack, Gavin	BSE - Inspector
Sirotko, Stephen	BSE - Inspector
Tabois, Alexander	BSE - Inspector

Visits:

Visit Date:	Temp:	Start Time:	End Time:
04/14/2021	58	08:10 AM	12:30 PM

58. DECK:

Reinforced Concrete Slab Deck with Embedded Steel Beams Overall Rating: 3

Rating

Overlay: 3	The bituminous concrete overlay in Spans 1, 2 & 3 has the following deficiencies: <ul style="list-style-type: none"> • Overlay is ± 1-1/2" thick, uneven and covered with sand. • Overlay is dull / hollow sounding. • Random bituminous patches with punky concrete and deteriorating bituminous up to 1" deep throughout. • Potholes throughout up to 5' long x 5' wide x up to 4" deep (any pothole greater than 1-1/2" deep are also spalls in the top of deck). <p>(See Sketch 2 and Photos 8 - 11)</p>
Deck - Str. Condition: 3	Refer to 'Girders' item below.
Curbs: 3	The right (south) curb is monolithic with the sidewalk. The left and right concrete curbs have the following deficiencies: <ul style="list-style-type: none"> • Spalling / severe scale up to full length x full height x full depth (6" deep) throughout. • Right curb spalls extend up to 6" into the concrete sidewalk. <p>Average curb reveals (taken at remaining curb areas): Left curb: 7" Right curb: 8"</p> <p>(See Sketch 2 and Photos 12 - 15)</p>
Median: N	
Sidewalks: 4	The right (south) concrete sidewalk has the following deficiencies: <ul style="list-style-type: none"> • Covered with sand and gravel. • Heavy debris accumulations. • Hollow areas throughout with up to 1-1/2" deep spalls with punky concrete x full length of bridge. • Areas of severe scale greater than 1" deep throughout with loose concrete along the edges. <p>Refer to 'Curbs' item above.</p> <p>(See Sketch 2 and Photos 13 & 15)</p>
Parapet: 3	The reinforced concrete parapets have the following deficiencies:

- Hairline to 1/16" wide map cracking with efflorescence.
- Spalling / severe scale / hollow areas up to full length x up to full width x up to 5" deep with and without exposed rebar, punky concrete, map cracking and efflorescence.
- Fascias / exteriors have areas of hairline to 1/8" wide map cracking with efflorescence throughout.

(See Sketch 2 and Photos 12, 13, 16 & 17)

Railing: 4 The painted-over steel angle bridge rails mounted on top of the parapets have the following deficiencies:

- Peeling paint throughout.
- Chips, scrapes and light rust throughout.
- Welded splices in the top rail are cracked at Pier 1, left and right rails, and at Pier 2, right rail only.

(See Sketch 2 and Photos 12, 13 & 18)

Paint: N

Fence: N

Drains: 5 The 4" diameter deck drains extending through the concrete slab have the following deficiency:

- Isolated drains are obstructed / clogged.

(See Sketch 2 and Photo 19)

Lighting Standard: N

Overall Utility Condition Rating N - Not Applicable

Utility Type/Size

N | No Utilities present

Construction Joints: N

Expansion Joint: N The deck joints over the abutments and piers are paved over.

Haunches Present over travelway? NO

APPROACH CONDITION:

Bituminous Concrete Approach Pavements

Overall Rating: 4

Rating

Approach Slab: N

Relief Joints: N

Approach Guide Rail: 4 The Southeast approach has no guide rail installed. The Southwest approach has a two-cable guide rail mounted on timber posts. The Northwest and Northeast approaches have metal beam guide rails on steel posts. The Southwest, Northwest and Northeast guide rails have the following deficiencies:

- Southwest approach guide rail has both cables missing; timber posts have checks up to 1/16" wide and rot up to 6" deep on the top.
- Northwest approach guide rail has (4) tilted posts and up to 4" long tears at all splices.
- Northeast approach guide rail has light surface rust and one (1) of five (5) bolts in place at the connection to the bridge (as-built condition).

(See Sketch 2 and Photos 20 - 24)

Approach Pavement: 5 The approach pavements have the following deficiencies:

- Map cracking up to 1/2" wide throughout.
- East approach has (4) potholes up to 45" diameter x 2" deep.
- East approach pavement is settled 1-1/2" at the bridge

(See Sketch 2 and Photos 25 & 26)

Approach Embankment: 8

Traffic Safety Features

Bridge Railings: 0 Open steel bridge railing does not meet Non-NHS criteria.

Transitions: 0 Northwest and Northeast approaches have no rub rails; transitions not stiffened.
Southwest approach cables not attached to parapet.
No guide rail at Southeast approach.

Approach Guardrails: 0 Does not meet R-B 350 criteria: no guide rail at Southeast approach, no blockouts used, timber guide rail at the northeast approach.

Approach Guardrail Ends: 0 Do not meet Non-NHS criteria.

59. SUPERSTRUCTURE:

Reinforced Concrete Slab with Embedded Steel Beams

Overall Rating: 3

Rating

Bearing Devices: N

Stringers: N

Girders: 3 The top surface of the slab is covered with a bituminous concrete overlay, however, the slab is exposed at potholes. Potholes greater than 1-1/2" deep are spalls in the slab (refer to 'Overlay' item above and Sketch 2 for top surface details).

The reinforced concrete slab with embedded steel beams has the following deficiencies:

Fascias:

- Areas of spalled concrete is typically punky.
- Hollow areas and hollow area / spalls up to full span length x full height (16") x up to 4" deep with (1) longitudinal exposed and debonded rebar with up to 40% section loss and punky concrete, with and without hairline map cracking and efflorescence, extending up to 1' under bridge.

Underside:

- Areas of spalled concrete is typically punky.
- Random areas of map cracking up to full span length with heavy efflorescence and stalactites.
- Random up to 3/16" wide longitudinal cracks up to full span length with and without heavy efflorescence and stalactites.
- Random areas of light scale/ honeycombing throughout.
- Concrete below steel beams with longitudinal cracks up to 1/8" wide with heavy efflorescence and stalactites and hollow areas up to full span length x up to 1' wide.
- Spalls and hollow areas up to full span length x up to 3'-3" wide x up to 3" deep with and without punky concrete, exposed rebar with laminated rust and up to 100% section loss, steel beam bottom flanges with up to laminated rust with up to 1/4" thick steel remaining (original flange thickness unknown).
- Spalls up to 3'-4" long x up to 2' wide x 3" deep around deck drains with punky concrete, exposed rebar with rust and up to 30% section loss.

(See Sketch 3 and Photos 27 - 34)

Floor Beams: N

Trusses - General: N

Trusses - Portals: N

Trusses - Bracing: N

Paint: N

Rust: 4 Refer to 'Girders' item above.

Machinery Movable Span: N

Rivets & Bolts:	N	
Welds - Cracks:	N	
Timber Decay:	N	
Concrete Cracking:	3	Refer to 'Girders' item above.
Collision Damage:	8	None noted.
Member Alignment:	8	
Deflection Under Load:	N	(N) Normal (E) Excessive
Vibration Under Load:	N	(N) Normal (E) Excessive
Stand Pipes:	N	
Catwalks:	N	
Movable Inspection System:	N	
Barrel Ladders:	N	

Are Barrel Ladders OSHA Compliant?

60. SUBSTRUCTURE:

Stone Masonry Abutments, Piers and Wingwalls
 Item 60. Substructure Overall Rating lowered from '4' to '3' based on areas with up to 100% mortar loss, missing stone and chinking stone voids with full depth penetrations.

Overall Rating:

Rating

Abutments - Stem:	<input type="text" value="3"/>	<p>The abutments have random patched and partially patched voids with some repointed masonry and the following deficiencies:</p> <ul style="list-style-type: none"> • Deteriorated mortar and missing chinking stones. • Intermittent areas of efflorescence up to full stem length extending down from the bearing seat. <p>Abutment 1:</p> <ul style="list-style-type: none"> • Random cracks up to 4' long x up to 1/4" wide with some extending through stones. • A 1' long x 1/2" wide crack with efflorescence extending from the bearing seat near the upstream (north) end. • Upstream (north) end has an 8' long open masonry joint along the water surface. • Up to 50% cracked/ missing mortar with missing chinking stones with up to 30" penetration extending from 1' above the waterline to channel bottom. • Random voids due to missing stones measuring up to 3.0' long x up to 1.0' high with up to 3.3' penetration. <p>Abutment 2:</p> <ul style="list-style-type: none"> • Random vertical cracks up to full-height x up to 1/4" wide. • Up to 20% mortar loss above the waterline with up to 1.0' penetration. • 100% mortar loss below the water surface with up to 1.0' penetration. • Random voids up to 3.0' long x 0.9' high with up to 6.2' penetration with and without loss of fill or roots growing. <p>Rating lowered from '4' to '3' based on areas with up to 100% mortar loss, missing stone and chinking stone voids with up to 6.2 penetrations.</p> <p>(See UW Drawings 3 & 8 and Photos 35 - 41)</p>
Abutments - Backwall:	N	Not visible.
Abutments - Footings:	N	The footings are not exposed.
Abutments - Settlement:	6	• Vertical cracks up to full-height x up to 1/4" wide in the abutments may indicate some settlement.
Abutments - Wingwalls:	3	The wingwalls have the following deficiencies:

	<ul style="list-style-type: none"> • Deteriorated / missing mortar and missing chinking stones. • Random voids up to 3' long x up to 1.5' high x up to 3.3' deep due to missing stones, some with vegetation growing. • Wingwall 1B has an 8' high x 2.5' wide area with random voids up to 10" high x 1' wide x 33" deep just above the waterline, and trees up to 10" diameter growing between stones. • Wingwall 2B has a 1' diameter tree growing between stones. <p>Rating lowered from '4' to '3' based on areas of mortar loss, missing stone and chinking stone voids with up to 3.3' penetration.</p> <p>(See UW Drawings 2, 3 & 8 and Photos 42 - 47)</p>
Piers/Bents - Caps: N	
Piers/Bents - Pile Bent: N	
Piers/Bents - Columns: 3	<p>The piers have random patched and partially patched voids and some repointed masonry, 6" wide step-outs on the upstream, west and east faces below the water surface, and the following deficiencies:</p> <ul style="list-style-type: none"> • Areas of up to severe efflorescence up to full pier length extending up to 3' down from the bearing seats. <p>Pier 1:</p> <ul style="list-style-type: none"> • Intermittent mortar cracks up to 1/4" wide from 1' above the water surface to the bearing seat. • Vertical cracks up to full-height x 1/4" wide through mortar and stones. • Up to 95% deteriorated / missing mortar with up to 3.0' (full pier depth) penetration extending from the waterline to channel bottom. • Loose and missing chinking stones up to 30% below the waterline with up to 1' penetration. • Random voids up to 0.8' long x 1.0' high x up to 3.0' deep (full pier depth). • A 2' high x 7" wide x 6" deep sheared stone at the northwest corner. • Upstream (north) nose has up to 100% missing mortar with up to 4.6' penetration, and a 2' long x 2" high separation of stone with up to 32" penetration. • Downstream (south) nose has a full-width x 2" high x 3' deep void along a joint between stones, and a missing mortar void with 1.5' penetration and active leakage. <p>Pier 2:</p> <ul style="list-style-type: none"> • Intermittent up to 1/4" wide cracks in mortar. • Random vertical cracks up to 2' long x up to 1/4" wide through mortar and stones. • Up to 75% deteriorated / missing mortar from 1' above the waterline to the channel bottom, with less than 5% deteriorated/ missing mortar with 15" penetration (typical) elsewhere. • Random voids up to 2.7' long x up to 1.0' high x up to 3.0' deep (full pier depth). • Southwest and southeast corners have up to 6" diameter trees growing, at southwest the tree is pushing out stone. <p>Rating lowered from '4' to '3' based on areas with up to 100% mortar loss, missing stone and chinking stone voids with up to full depth penetrations.</p> <p>(See UW Drawings 2 & 4 - 7 and Photos 48 - 60)</p>
Piers/Bents - Footings: N	The pier footings are not exposed.
Piers/Bents - Settlement: 6	• Vertical cracks up to full-height x up to 1/4" wide in the piers may indicate some settlement.
Erosion - Scour: 7	<p>Erosion - '7'</p> <p>None noted.</p> <p>Scour - '7'</p> <p>Channel bottom elevations around the Abutments and Piers have the following variations greater than 1.0' as compared to the 2020 Inspection:</p>

	<ul style="list-style-type: none"> Isolated areas of degradation up to 1.4' deep at Pier 1 and Pier 2. <p>(See UW Drawings 1 - 8)</p>
Concrete Crack - Spall:	N
Steel Corrosion:	N
Paint:	N
Timber Decay:	N
Collision Damage:	8 None noted.
Debris:	7 <ul style="list-style-type: none"> Minor timber and man-made debris. <p>(See UW Drawing 1)</p>

61. CHANNEL AND CHANNEL PROTECTION:

The Pachaug River is a freshwater body that flows from north to south beneath the bridge. Overall Rating: 6

Rating

Channel - Scour:	7	<p>Channel bottom elevations along the fascias have no variations greater than 1.0' as compared to the 2020 Inspection.</p> <p>No significant aggradation or degradation was noted.</p> <p>Refer to 60. 'Erosion - Scour' item above for channel bottom elevation details along the substructure units.</p> <p>(See UW Drawing 1 and Photos 61 & 62)</p>
Embankment - Erosion:	6	<p>The downstream (south) embankments have dry-laid stone retaining walls.</p> <ul style="list-style-type: none"> Upstream (north) embankments have minor erosion with undercutting. <p>(See UW Drawing 1 and Photos 42, 43, 45, 47, 61 & 62)</p>
Debris:	6	<ul style="list-style-type: none"> Minor timber and man-made debris in the channel. A 15' long x 4' wide area of aggradation (island) in the downstream (south) channel with up to 1.5' diameter trees growing. <p>(See UW Drawing 1 and Photo 62)</p>
Vegetation:	7	<p>The embankments are well vegetated.</p> <p>(See Photos 42, 43, 45, 47, 61 & 62)</p>
Channel Change:	7	<p>No apparent change in the channel orientation.</p> <p>The Pachaug River flows in from the northeast to the bridge.</p> <p>There is a stepped spillway directly downstream (south) of the bridge.</p> <p>(See UW Drawing 1 and Photos 61 & 62)</p>
Fender - System:	N	
Spur Dikes and Jetties:	N	
Rip Rap:	N	

62. CULVERTS AND RETAINING WALLS:

Overall Rating: N

Form: BRI-18, Rev. 1/14
 Inspection type: Routine, Underwater
 Inspection Date: 4/14/2021
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:Bridge No 04671

Town: GRISWOLD
 Carried: CARROLL ROAD
 Crossed: PACHAUG RIVER
 Inventory Route: Non-NHS

Rating

Barrel:	
Concrete:	
Steel:	
Timber:	
Headwall:	
Cutoff Wall:	
Debris:	
Retaining Wall System:	
Footings:	

LOAD POSTING:

Rating

Single Unit (Tons):	
Semi Trailer (Tons):	
4 Axle (Tons):	
3S2 (Tons):	
All Vehicles:	15
Advanced Warning:	All Approaches West approach load posting is on Carroll Rd after turn from Sheldon Rd. near bridge (See Photo 63). East Approach load posting is at intersection of Carroll Row & Lillibridge Ave. near the Voluntown town line (See Photo 64).
Warning At Bridge:	Some Approaches See Advanced Warning.
Legibility:	Adequate
Visibility:	Adequate

VERTICAL CLEARANCE POSTING

Min. Vert Under Clearance:		Ft		In	Structure spans a waterway.
Posted Clearance Under Bridge:		Ft		In	
Posted Clearance On Bridge:		Ft		In	
Advanced Warning:					
Warning At Bridge:					
Legibility:					
Visibility:					

NOTES / COMMENTS:

Character of Traffic: Light volume with mostly light weights.

Additional Notes:

- The Bridge Identification Number is in-place and legible.
- The bridge is logged from west to east consistent with the previous report.
- Routine and Underwater inspections were performed concurrently.
- Per CTDOT BS&E, the bridge is to be coded as a concrete slab, not multi-girder bridge.
- There are two (2) open Maintenance Work Items associated with the 2018 Inspection Report: 04671-2018-0003 & 0004 have been assigned to bridge (ATB).
- There are two (2) open Maintenance Work Items associated with the 2017 Inspection Report: 04671-2017-0001 & 0002 have been assigned to town (TWN).

Form: BRI-18, Rev. 1/14

Inspection type: Routine,Underwater

Inspection Date: 4/14/2021

Inspected by: CONSOR Engineers

:Bridge No 04671

Town: GRISWOLD

Carried: CARROLL ROAD

Crossed: PACHAUG RIVER

Inventory Route: Non-NHS

Additional Comments:

Phone call to Town (Todd Babbitt) on 5-7-21 in place of 2021 Town Letter. The Town will proceed with the structure removal Proj.#57-121 under the Federal Local Bridge Program with a signed design agreement April 2021, Est. construction 2022

Any previous submitted Work items deleted 2021. BS&E no longer requires work Items for Town maintenance responsibilities

National Bridge Elements
Inspection type: Routine, Underwater
Inspection Date: 4/14/2021
Inspected by: CONSOR Engineers

:Bridge No 04671

Town: GRISWOLD
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Crossed: PACHAUG RIVER
Inventory Route: Non-NHS

	Environment	Total Quantity	Units	Condition State 1	Condition State 2	Condition State 3	Condition State 4
38 - Reinforced Concrete Slab	Mod.	2400	sq. ft.	600	554	1206	40
1080 - Delamination/Spall/Patched Area		567		0	201	326	40
1090 - Exposed Rebar		357		0	0	357	0
1120 - Efflorescence/Rust Staining		544		0	163	381	0
1130 - Cracking (RC and Other)		332		0	190	142	0
510 - Wearing Surfaces		1740	sq. ft.	185	1050	424	81
3210 - Delamination/Spall/Patched Area/Pothole (Wearing Surfaces)		261		0	172	89	0
3220 - Crack (Wearing Surface)		639		0	379	260	0
3230 - Effectiveness (Wearing Surface)		655		0	499	75	81
213 - Masonry Pier Wall	Mod.	56	ft.	0	6	50	0
1120 - Efflorescence/Rust Staining		8		0	3	5	0
1610 - Mortar Breakdown (Masonry)		23		0	0	23	0
1620 - Split/Spall (Masonry)		18		0	0	18	0
1630 - Patched Area (Masonry)		2		0	2	0	0
1640 - Masonry Displacement		5		0	1	4	0
217 - Masonry Abutment	Mod.	56	ft.	0	13	43	0
1120 - Efflorescence/Rust Staining		21		0	8	13	0
1610 - Mortar Breakdown (Masonry)		16		0	0	16	0
1620 - Split/Spall (Masonry)		11		0	0	11	0
1630 - Patched Area (Masonry)		3		0	3	0	0
1640 - Masonry Displacement		5		0	2	3	0
330 - Metal Bridge Railing	Mod.	174	ft.	79	92	0	3
1000 - Corrosion		92		0	92	0	0
1020 - Connection		3		0	0	0	3
515 - Steel Protective Coating		248	sq. ft.	0	149	0	99
3440 - Effectiveness (Steel Protective Coatings)		248		0	149	0	99
331 - Reinforced Concrete Bridge Railing	Mod.	174	ft.	0	31	143	0
1080 - Delamination/Spall/Patched Area		126		0	18	108	0
1090 - Exposed Rebar		16		0	3	13	0
1120 - Efflorescence/Rust Staining		23		0	10	13	0
1130 - Cracking (RC and Other)		9		0	0	9	0

Form: BRI-58.

Inspection type: Routine, Underwater

Inspection Date: 4/14/2021

Inspected by: CONSOR Engineers

:Bridge No 04671

Town: GRISWOLD

Carried: CARROLL ROAD

Crossed: PACHAUG RIVER

Inventory Route: Non-NHS

UNDERWATER INSPECTION

ITEM	RATING	REMARKS
60. SUBSTRUCTURE	3	Stone Masonry Abutments, Piers and Wingwalls Item 60. Substructure Overall Rating lowered from '4' to '3' based on areas with up to 100% mortar loss, missing stone and chinking stone voids with full depth penetrations.
ABUTMENT 1:	3	West Abutment.
STEM	3	The abutments have random patched and partially patched voids with some repointed masonry and the following deficiencies: <ul style="list-style-type: none"> • Deteriorated mortar and missing chinking stones. • Intermittent areas of efflorescence up to full stem length extending down from the bearing seat. <p>Abutment 1:</p> <ul style="list-style-type: none"> • Random cracks up to 4' long x up to 1/4" wide with some extending through stones. • A 1' long x 1/2" wide crack with efflorescence extending from the bearing seat near the upstream (north) end. • Upstream (north) end has an 8' long open masonry joint along the water surface. • Up to 50% cracked/ missing mortar with missing chinking stones with up to 30" penetration extending from 1' above the waterline to channel bottom. • Random voids due to missing stones measuring up to 3.0' long x up to 1.0' high with up to 3.3' penetration. <p>Rating lowered from '4' to '3' based on areas with up to 100% mortar loss, missing stone and chinking stone voids with up to 3.3 penetration.</p> <p>(See UW Drawing 3 and Photos 35 - 37)</p>
FOOTING	N	Not exposed.
EROSION	7	No significant erosion observed.
SETTLEMENT	6	<ul style="list-style-type: none"> • Vertical cracks up to 3' high x up to 1/4" wide in the abutment may indicate some settlement.
SCOUR	7	Channel bottom elevations along Abutment 1 have no variations greater than 1.0' as compared to the 2020 Inspection. (See UW Drawings 1 - 8)
WINGWALLS	3	The wingwalls have the following deficiencies: <ul style="list-style-type: none"> • Deteriorated / missing mortar and missing chinking stones. • Random voids up to 3' long x up to 1.5' high x up to 3.3' deep due to missing stones, some with vegetation growing. • Wingwall 1B has an 8' high x 2.5' wide area with random voids up to 10" high x 1' wide x 33" deep just above the waterline, and trees up to 10" diameter growing between stones. <p>Rating lowered from '4' to '3' based on areas of mortar loss, missing stone and chinking stone voids with up to 3.3' penetration.</p> <p>(See UW Drawings 2 & 3 and Photos 42 - 44)</p>

General Remarks:

ABUTMENT 2:	3	East Abutment.
STEM	3	The abutments have random patched and partially patched voids with some repointed masonry and the following deficiencies: <ul style="list-style-type: none"> • Deteriorated mortar and missing chinking stones. • Intermittent areas of efflorescence up to full stem length extending down from the bearing seat. <p>Abutment 2:</p>

Form: BRI-58.

Inspection type: Routine, Underwater

Inspection Date: 4/14/2021

Inspected by: CONSOR Engineers

Bridge No 04671

Town: GRISWOLD

Carried: CARROLL ROAD

Crossed: PACHAUG RIVER

Inventory Route: Non-NHS

- Random vertical cracks up to full-height x up to 1/4" wide.
- Up to 20% mortar loss above the waterline with up to 1.0' penetration.
- 100% mortar loss below the water surface with up to 1.0' penetration.
- Random voids up to 3.0' long x 0.9' high with up to 6.2' penetration with and without loss of fill or roots growing.

Rating lowered from '4' to '3' based on areas with up to 100% mortar loss, missing stone and chinking stone voids with up to 6.2 penetrations.

(See UW Drawing 8 and Photos 38 - 41)

FOOTING N

Not exposed.

EROSION 7

No significant erosion observed.

SETTLEMENT 6

• Vertical cracks up to full-height x up to 1/4" wide in the abutment may indicate some settlement.

SCOUR 7

Channel bottom elevations along Abutment 2 have no variations greater than 1.0' as compared to the 2020 Inspection.

(See UW Drawings 1 - 8)

WINGWALLS 3

The wingwalls have the following deficiencies:

- Deteriorated / missing mortar and missing chinking stones.
- Random voids up to 3' long x up to 1.5' high x up to 3.3' deep due to missing stones, some with vegetation growing.
- Wingwall 2B has a 1' diameter tree growing between stones.

Rating lowered from '4' to '3' based on areas of mortar loss, missing stone and chinking stone voids with up to 3.3' penetration.

(See UW Drawings 2 & 8 and Photos 45 - 47)

General Remarks:

PIER NO. 1 3

West Pier.

PILES N

STEM 3

The piers have random patched and partially patched voids and some repointed masonry, 6" wide step-outs on the upstream, west and east faces below the water surface, and the following deficiencies:

- Areas of up to severe efflorescence up to full pier length extending up to 3' down from the bearing seats.

Pier 1:

- Intermittent mortar cracks up to 1/4" wide from 1' above the water surface to the bearing seat.
- Vertical cracks up to full-height x 1/4" wide through mortar and stones.
- Up to 95% deteriorated / missing mortar with up to 3.0' (full pier depth) penetration extending from the waterline to channel bottom.
- Loose and missing chinking stones up to 30% below the waterline with up to 1' penetration.
- Random voids up to 0.8' long x 1.0' high x up to 3.0' deep (full pier depth).
- A 2' high x 7" wide x 6" deep sheared stone at the northwest corner.
- Upstream (north) nose has up to 100% missing mortar with up to 4.6' penetration, and a 2' long x 2" high separation of stone with up to 32" penetration.
- Downstream (south) nose has a full-width x 2" high x 3' deep void along a joint between stones, and a missing mortar void with 1.5' penetration and active leakage.

Rating lowered from '4' to '3' based on areas with up to 100% mortar loss, missing stone and chinking stone voids with up to full depth penetrations.

(See UW Drawings 2, 4 & 5 and Photos 48 - 53)

FOOTING N

Not exposed.

SCOUR 7

Channel bottom elevations around the Piers have the following variations greater than 1.0' as

05/25/2021

Page 2 of 4

Form: BRI-58.

Inspection type: Routine, Underwater

Inspection Date: 4/14/2021

Inspected by: CONSOR Engineers

:Bridge No 04671

Town: GRISWOLD

Carried: CARROLL ROAD

Crossed: PACHAUG RIVER

Inventory Route: Non-NHS

compared to the 2020 Inspection:

- Isolated areas of degradation up to 1.4' deep at Pier 1 and Pier 2.

(See UW Drawings 1 - 8)

- Vertical cracks up to full-height x up to 1/4" wide in the piers may indicate some settlement.

SETTLEMENT 6

General Remarks:

PIER NO. 2 3

East Pier.

PILES N

STEM 3

The piers have random patched and partially patched voids and some repointed masonry, 6" wide step-outs on the upstream, west and east faces below the water surface, and the following deficiencies:

- Areas of up to severe efflorescence up to full pier length extending up to 3' down from the bearing seats.

Pier 2:

- Intermittent up to 1/4" wide cracks in mortar.
- Random vertical cracks up to 2' long x up to 1/4" wide through mortar and stones.
- Up to 75% deteriorated / missing mortar from 1' above the waterline to the channel bottom, with less than 5% deteriorated / missing mortar with 15" penetration (typical) elsewhere.
- Random voids up to 2.7' long x up to 1.0' high x up to 3.0' deep (full pier depth).
- Southwest and southeast corners have up to 6" diameter trees growing, at southwest the tree is pushing out stone.

Rating lowered from '4' to '3' based on areas with up to 100% mortar loss, missing stone and chinking stone voids with up to full depth penetrations.

(See UW Drawings 2, 6 & 7 and Photos 54 - 60)

FOOTING N

Not exposed.

SCOUR 7

Channel bottom elevations around the Piers have the following variations greater than 1.0' as compared to the 2020 Inspection:

- Isolated areas of degradation up to 1.4' deep at Pier 1 and Pier 2.

(See UW Drawings 1 - 8)

SETTLEMENT 7

- Vertical cracks up to 2' long x up to 1/4" wide in the piers may indicate some settlement.

General Remarks:

61. CHANNEL & CHANNEL PROTECTION:

6

The Pachaug River is a freshwater body that flows from north to south beneath the bridge.

CHANNEL SCOUR 7

Channel bottom elevations along the fascias have no variations greater than 1.0' as compared to the 2020 Inspection.

No significant aggradation or degradation was noted.

Refer to 60. 'Scour' items above for channel bottom elevation details along the substructure units.

(See UW Drawing 1 and Photos 61 & 62)

EMBANKMENT EROSION 6

The downstream (south) embankments have dry-laid stone retaining walls.

- Upstream (north) embankments have minor erosion with undercutting.

Form: BRI-58.

Inspection type: Routine, Underwater

Inspection Date: 4/14/2021

Inspected by: CONSOR Engineers

:Bridge No 04671

Town: GRISWOLD

Carried: CARROLL ROAD

Crossed: PACHAUG RIVER

Inventory Route: Non-NHS

(See UW Drawing 1 and Photos 42, 43, 45, 47, 61 & 62)

DEBRIS 6

- Minor timber and man-made debris in the channel.
- A 15' long x 4' wide area of aggradation (island) in the downstream (south) channel with up to 1.5' diameter trees growing.

(See UW Drawing 1 and Photo 62)

VEGETATION 7

The embankments are well vegetated.

(See Photos 42, 43, 45, 47, 61 & 62)

CHANNEL CHANGE 7

No apparent change in the channel orientation.

The Pachaug River flows in from the northeast to the bridge.

There is a stepped spillway directly downstream (south) of the bridge.

(See UW Drawing 1 and Photos 61 & 62)

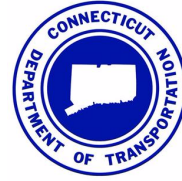
FENDER SYSTEM N

SPUR DIKES & JETTIES N

RIP RAP N

General Remarks:

UNDERWATER INSPECTION (BRI-59)



Bridge: 04671 Town: 33900 - GRISWOLD Carried: CARROLL ROAD Crossed: PACHAUG RIVER

Total Number of Piers: 2

Piers in the Water: 2

Boat Used? No

Boat Size: N/A

Dive Station Used? No

Access to Bridge: Shore

Access/ Equipment Comments U/W: Wade with drysuit.

Type of water: Fresh

Marine Growth: Light algae

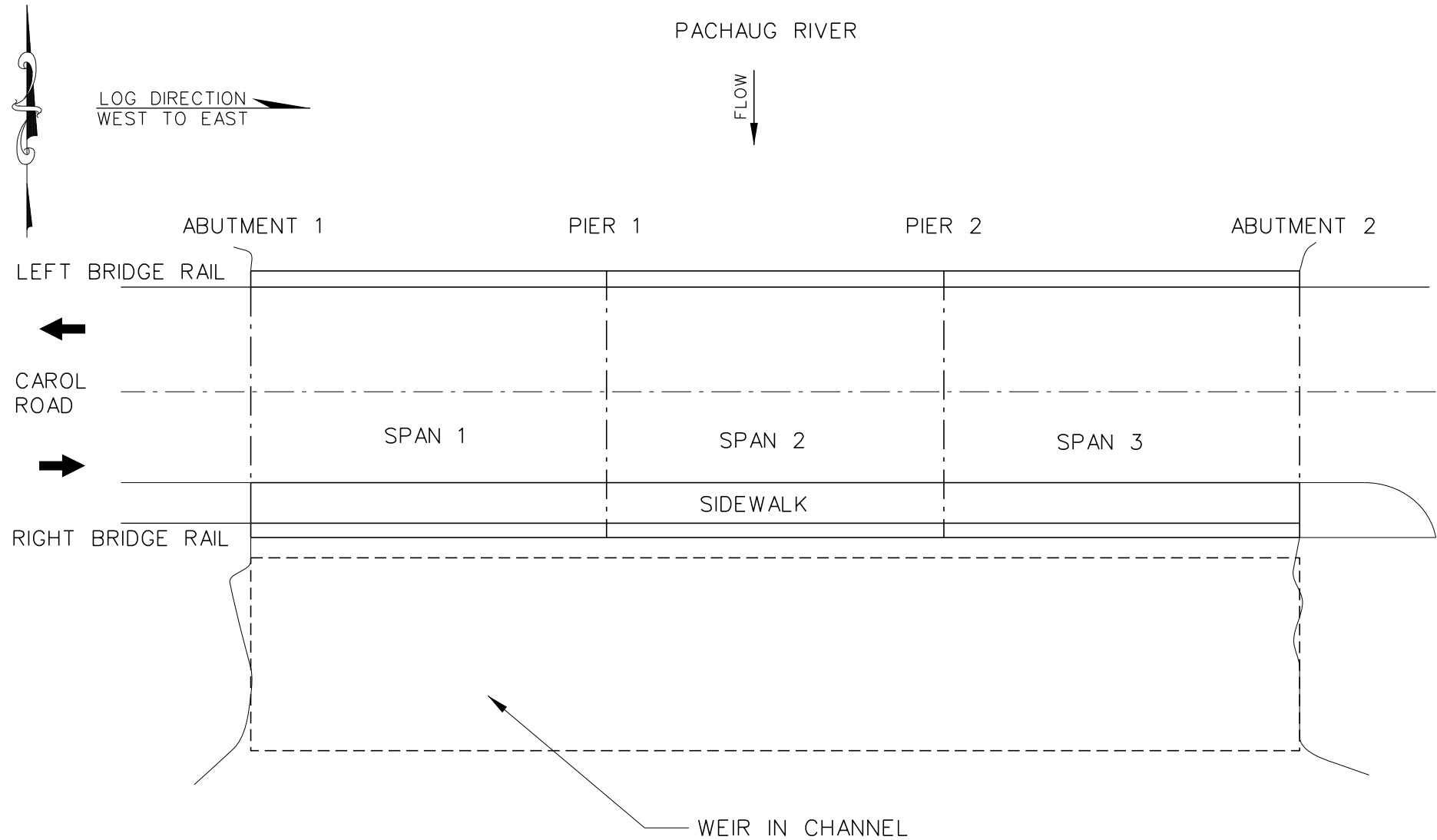
Max Water Depth: 4.4'

Max Water Depth at Pier Or Abutment: 4.4'


Current Strength: 1-2 fps

U/W Visibility: 3.0'

Bottom Composition: Cobbles, gravel, sand, silt and large river rock with up to 1.0' penetration into the channel bottom.

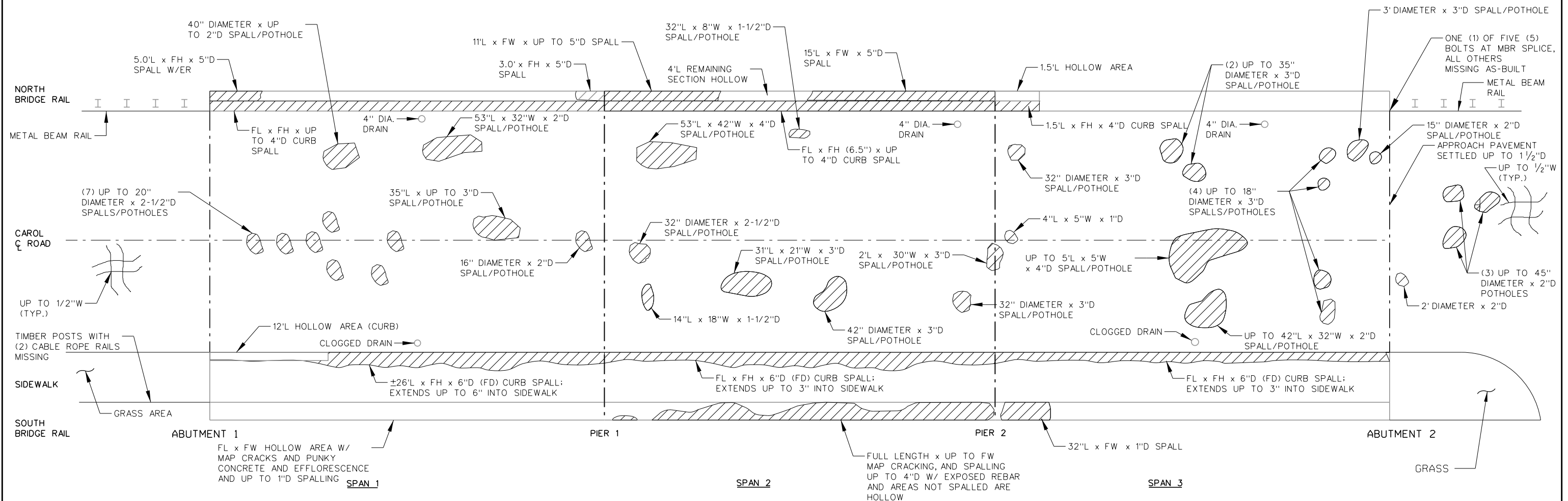


KEY PLAN
N.T.S.

 <p>50 Inwood Road, Suite 101 Rocky Hill, CT 06067 PH.: 860.840.2505</p> <p>CONSOR</p>	CONNECTICUT DEPARTMENT OF TRANSPORTATION			
	BRIDGE NO. 04671			
	CAROL ROAD OVER PACHAUG RIVER			
	GRISWOLD			CONNECTICUT
KEY PLAN				
INSPECTED: SS, MB, AT, GM	SCALE: AS SHOWN	DATE OF INSPECTION: 04 / 14 / 21	DRAWING NO. 04671A	
REVISED BY: FH				

SKETCH 1

LOG DIRECTION
WEST TO EAST



NOTES:

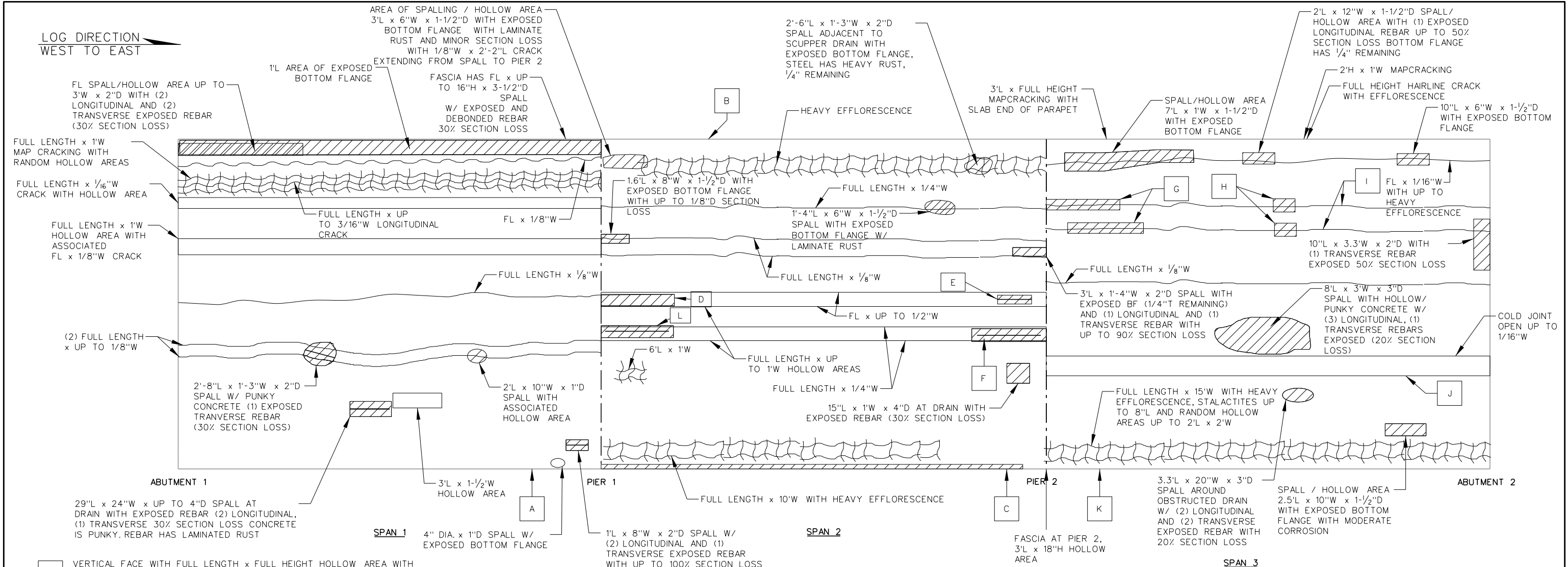
1. BITUMINOUS OVERLAY IS UNEVEN AND COVERED WITH SAND, OVERLAY IS $\pm 1\frac{1}{2}$ " THICK, ANY POTHOLE $> 1\frac{1}{2}$ " DEEP IS ALSO A SPALL IN THE DECK. OVERLAY IS TYPICALLY DULL/HOLLOW SOUNDING WITH RANDOM PATCHES/PUNKY CONCRETE AND TRANSVERSE AND HORIZONTAL CRACKS UP TO $\frac{1}{4}$ " WIDE IN ALL SPANS. POTHOLES PATCHED W/ DETERIORATING BITUMINOUS THROUGHOUT.
2. PARAPETS HAVE HAIRLINE TO $\frac{1}{16}$ " WIDE MAP CRACKS W/ EFFLORESCENCE AND SEVERE SCALE.
3. TWO-CABLE GUIDE RAIL IN SOUTHWEST APPROACH HAS NO CABLES, TIMBER POSTS HAVE CHECKS UP TO $\frac{1}{16}$ " WIDE AND ROT UP TO 6" DEEP ON TOP.
4. PAINTED STEEL BRIDGE RAILINGS HAVE PEELING PAINT, CHIPS, SCRAPES, AND LIGHT RUST.
5. SIDEWALK HAS AREAS OF SEVERE SCALE (> 1 " DP). LOOSE CONCRETE ALONG EDGES, AND IS COVERED WITH SAND AND GRAVEL.
6. RANDOM BITUMINOUS POTHOLES WITH DETERIORATION (UP TO 1"D).
7. TOP OF RAIL WELDED SPLICE CRACKED OR SEPARATED AND PAINTED OVER AT PIER 1 BOTH NORTH AND SOUTH RAILS AND SOUTH RAIL ONLY AT PIER 2.
8. NORTHEAST APPROACH GUIDE RAIL HAS LIGHT SURFACE RUST.
9. NORTHWEST APPROACH GUIDE RAIL HAS (4) TITLED POSTS AND UP TO 4" LONG TEARS AT ALL SPLICES.
10. THERE IS NO GUIDERAIL AT THE SOUTHEAST APPROACH.
11. PARAPET EXTERIORS HAVE AREAS OF HAIRLINE TO $\frac{1}{8}$ " WIDE MAPCRACKING WITH EFFLORESCENCE THROUGHOUT.

TOP OF DECK
N.T.S.

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CONNECTICUT DEPARTMENT OF TRANSPORTATION			
BRIDGE NO. 04671 CAROL ROAD OVER PACHAUG RIVER			
GRISWOLD	CONNECTICUT		SKETCH 2
TOP OF DECK			
INSPECTED: SS, MB, AT, GM REVISED BY: FH	SCALE: AS SHOWN	DATE OF INSPECTION 04 / 14 / 21	DRAWING NO. 04671B

LOG DIRECTION
WEST TO EAST



UNDERSIDE OF SLAB
N.T.S.

- A** VERTICAL FACE WITH FULL LENGTH x FULL HEIGHT HOLLOW AREA WITH FAILED PATCHES, HAIRLINE MAP CRACKS, EFFLORESCENCE AND UP TO 2"D SPALLS WITH PUNKY CONCRETE.
- B** FASCIA WITH FULL LENGTH x UP TO FULL HEIGHT HOLLOW AREA WITH UP TO 4"D SPALLING WITH PUNKY CONCRETE AND EFFLORESCENCE, (1) EXPOSED AND DEBONDED LONGITUDINAL REBAR WITH 30% SECTION LOSS; EXTENDS UNDER BRIDGE 1'W
- C** VERTICAL FACE OF SLAB WITH FULL LENGTH x 16"H x 3"D SPALLS/SCALE/HOLLOW AREA WITH PUNKY CONCRETE WITH (1) LONGITUDINAL EXPOSED REBAR (10% SECTION LOSS) EXTENDS UNDER FASCIA 8"W.
- D** HOLLOW AREA 3.3'L x 10"W x 1"D SPALL WITH EXPOSED BOTTOM FLANGE WITH LAMINATE RUST (1/8"D SECTION LOSS).
- E** AREA OF SPALLING / HOLLOW AREA WITH EXPOSED BOTTOM FLANGE 6.5'L x 10"W x 1-1/2"D WITH 1/2" T LAMINATE RUST WITH UP TO 1/4"D SECTION LOSS.
- F** 9'L x UP TO 1'-6"W x 1-1/2"D SPALL W/EXPOSED BOTTOM FLANGE AND 1/2" T LAMINATED RUST WITH UP TO 1/4"D SECTION LOSS.
- G** 7'L x 6"W x 1-1/2"D SPALL W/ EXPOSED BOTTOM FLANGE AND EFFLORESCENCE.
- H** 1.3'L x 8"W x 1-1/2"D SPALL WITH EXPOSED BOTTOM FLANGE
- I** FULL LENGTH 1/16"W LONGITUDINAL CRACK WITH EFFLORESCENCE
- J** FULL LENGTH UP TO 1/8"W LONGITUDINAL CRACK WITH HEAVY EFFLORESCENCE, AS WELL AS MAP CRACKING, AND HOLLOW AREAS UP TO 1'W
- K** FULL LENGTH x UP TO 18"H x 3"D SPALL/HOLLOW AREA WITH EXPOSED REBAR WITH ISOLATED AREAS UP TO 40% SECTION LOSS AT VERTICAL FACE OF SLAB AND EXTENDS UNDER SLAB 10"W. REMAINING FACE HAS HAIRLINE CRACKS WITH EFFLORESCENCE.

L SPALLING/HOLLOW AREA 8'L x 9"W x 1-1/2"D WITH EXPOSED BOTTOM FLANGE WITH HEAVY CORROSION UP TO 1/8"D SECTION LOSS.

- NOTES:**
1. HAIRLINE CRACKS UP TO FULL LENGTH WITH EFFLORESCENCE THROUGHOUT.
 2. SPALLED CONCRETE IS TYPICALLY PUNKY, ESPECIALLY AT FASCIAS.
 3. LAMINATED RUST ON EXPOSED BOTTOM FLANGES WITH SECTION LOSS UP TO 1/4"D UNLESS OTHERWISE NOTED.
 4. RANDOM SPALLS UP TO 1'DIA x 1-1/2"D AND AREAS OF LIGHT SCALE AND HONEYCOMBING ON SLAB UNDERSIDE.
 5. HEAVY EFFLORESCENCE AND STALACTITES UP TO 8"L ALONG CRACKS.
 6. DRAINS NOT SHOWN UNLESS DEFECTIVE.
 7. ENCASED BEAMS SPACED 3.5' APART.
 8. CONCRETE BELOW ENCASED BEAMS HAVE LONGITUDINAL CRACKS UP TO 1/8"W WITH HEAVY EFFLORESCENCE AND HOLLOW AREAS.
 9. SPALL LESS THAN 1'DIA. HAVE BEEN PARTIALLY PATCHED ON SLAB UNDERSIDE.

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CONSOR

CONNECTICUT DEPARTMENT OF TRANSPORTATION

BRIDGE NO. 04671
CAROL ROAD OVER PACHAUG RIVER

GRISWOLD CONNECTICUT

UNDERSIDE OF SLAB

INSPECTED: SS, MB, AT, GM
REVISED BY: FH

SCALE: AS SHOWN

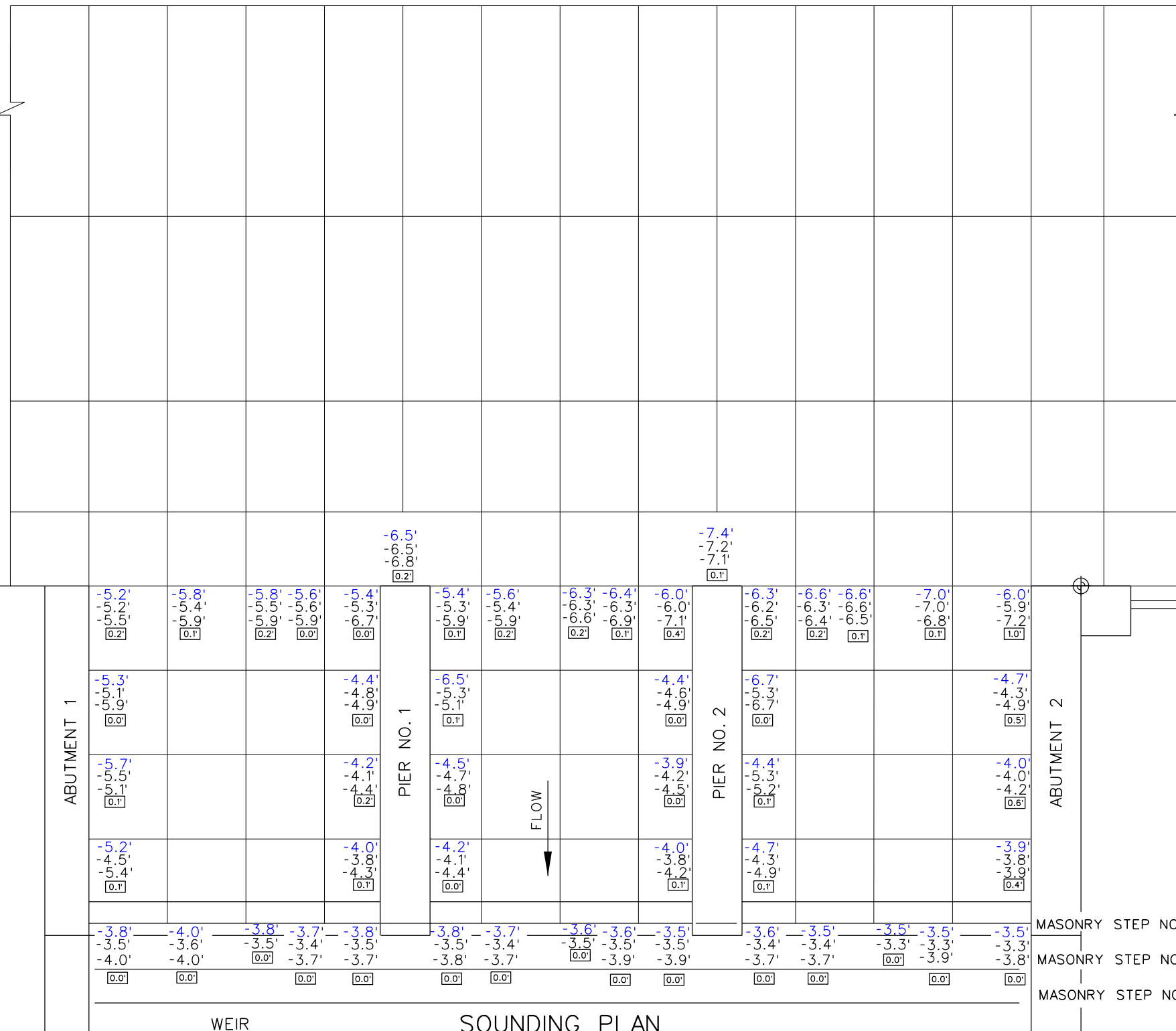
DATE OF INSPECTION: 04 / 14 / 21

DRAWING NO. 04671C

SKETCH 3

LOG DIRECTION
WEST TO EAST

6.5' ± 10.0' ± 3.0' ± 3.0' ± 10.0' ± 10.0' ± 3.0' ± 3.0' ± 10.0' ± 10.0' ± 3.0' ± 3.0' ± 10.0' ± 6.5' ± 6.5' ±



50'
25'
15'
10'

FLOW

SHORELINE (TYPICAL)

STONE RETAINING WALL

SHORELINE (TYPICAL)

ABUTMENT 1

PIER NO. 1

PIER NO. 2

ABUTMENT 2

FLOW

MASONRY STEP NO. 1, ELEV. -3.8' ±
MASONRY STEP NO. 2, ELEV. -4.7'
MASONRY STEP NO. 3, ELEV. -5.1'

NOTES:

- UPSTREAM (NORTH) CHANNEL EMBANKMENTS HAVE MINOR EROSION/UNDERCUTTING.
- DOWNSTREAM (SOUTH) CHANNEL EMBANKMENTS HAVE DRY-LAID STONE RETAINING WALLS.
- THERE IS A 15'L x 4'W ISLAND WITH TREES UP TO 1.5'φ IN THE DOWNSTREAM (SOUTH) CHANNEL DIRECTLY SOUTH OF THE DAM/SPILLWAY.
- MINOR TIMBER AND MAN-MADE DEBRIS WAS OBSERVED IN THE CHANNEL.

LEGEND FOR BOTTOM ELEVATIONS

APR. 2021: -0.0'
APR. 2020: -0.0'
FEB. 2019: -0.0'

SOUNDING PLAN

N.T.S.

LEGEND FOR SYMBOLS

⊕ DATUM ELEV. 0.0' TAKEN FROM THE TOP OF WINGWALL 2A, AT ABUTMENT 2.

▽ W.S. ELEV. -3.0'
-2.8'
-2.9'

□ DEPTH OF PENETRATION

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CONSOR

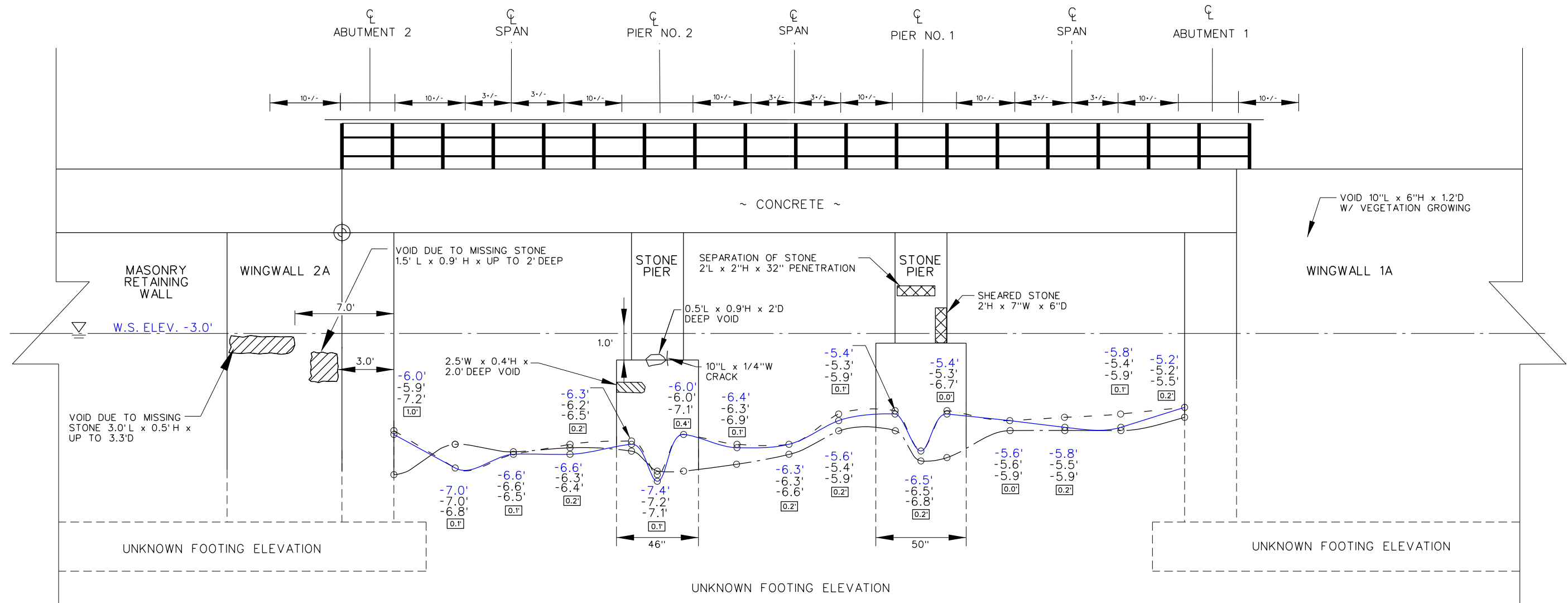
CONNECTICUT DEPARTMENT OF TRANSPORTATION

BRIDGE NO. 04671
CAROL ROAD OVER PACHAUG RIVER
CONNECTICUT

SOUNDING PLAN

INSPECTED: SS, MB, AT, GM
REVISOR: FH
SCALE: AS SHOWN
DATE OF INSPECTION: 04 / 14 / 21
DRAWING NO. 04671 D

UW DRAWING 1



UPSTREAM (NORTH) ELEVATION
N.T.S.

LEGEND FOR BOTTOM ELEVATIONS

APR. 2021: -0.0'
APR. 2020: -0.0'
FEB. 2019: -0.0'

LEGEND FOR MUDLINE CONTOURS

APR. 2021
APR. 2020
FEB. 2019

LEGEND FOR SYMBOLS

⊕ DATUM ELEV. 0.0' TAKEN FROM THE TOP OF WINGWALL 2A, AT ABUTMENT 2.

▽ W.S. ELEV. -3.0'
-2.8'
-2.9'

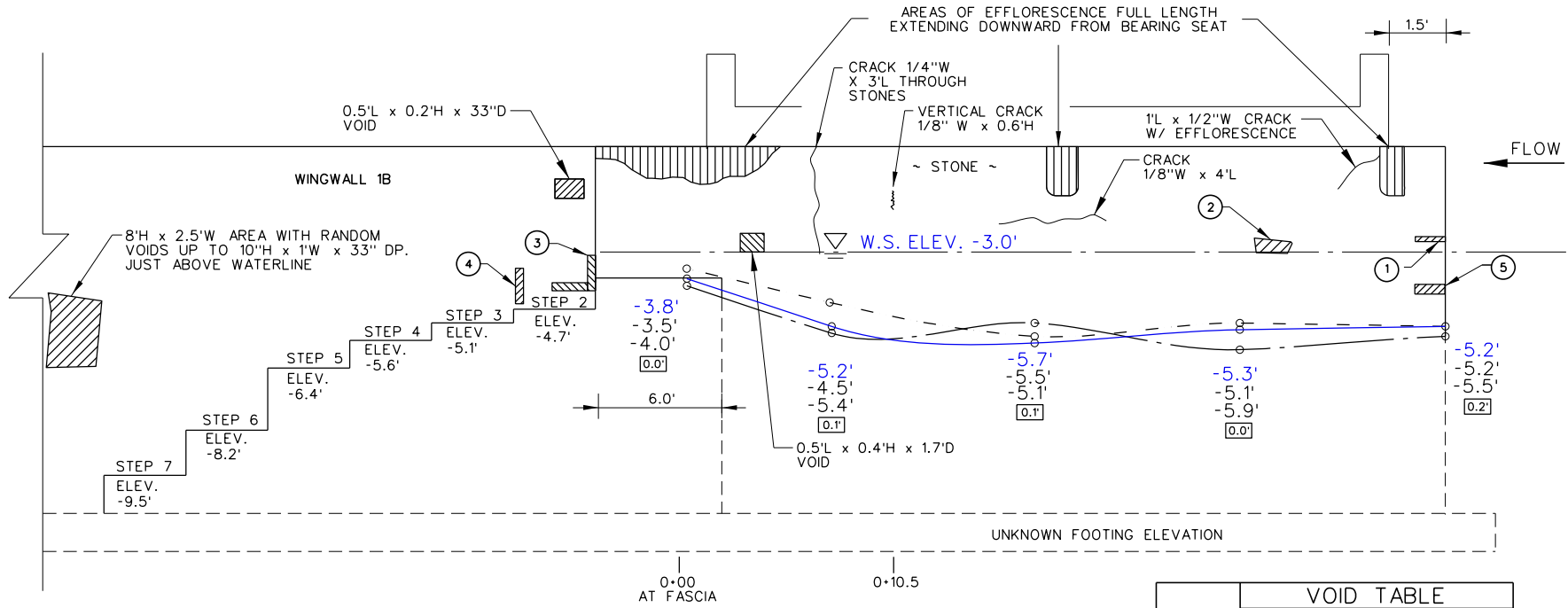
0.0 DEPTH OF PENETRATION

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CONSOR

CONNECTICUT DEPARTMENT OF TRANSPORTATION			
BRIDGE NO. 04671 CAROL ROAD OVER PACHAUG RIVER			
GRISWOLD			CONNECTICUT
UPSTREAM (NORTH) ELEVATION			
INSPECTED: SS, MB, AT, GM	SCALE: AS SHOWN	DATE OF INSPECTION: 04 / 14 / 21	DRAWING NO. 04671 E
REVISED BY: FH			

UW DRAWING 2



NOTES

- MISSING CRACKED MORTAR AND CHINKING STONES, UP TO 50% EXTENDING FROM 1.0' ABOVE THE WATERLINE TO THE CHANNEL BOTTOM. PENETRATION UP TO 30".
- MASONRY JOINT AT WATER SURFACE AT NORTH END OF ABUTMENT IS OPEN 8"L.
- RANDOM VOIDS AT WINGWALL 1A UP TO 8"L x 8"H x 2"D.
- TREE UP TO 10" DIAMETER GROWING OUT OF WINGWALL 1B.
- SEVERAL VOIDS HAVE BEEN PATCHED AND SOME MASONRY HAS BEEN REPOINTED.
- RANDOM CRACKS UP TO 3'L x 1/4"W.

ABUTMENT 1
N.T.S.

VOID NO.	VOID TABLE		
	ENGLISH DIMENSIONS		
	LENGTH	VERTICAL HEIGHT	PENETRATION
①	2.3'	0.4'	1.3'
②	0.5'	0.8'	3.3'
③	1.7'	1.5'	2.1'
④	0.3'	0.8'	2.5'
⑤	3.0'	1.0'	2.0'

LEGEND FOR SYMBOLS

- ⊕ DATUM ELEV. 0.0' TAKEN FROM THE TOP OF WINGWALL 2A, AT ABUTMENT 2.
- ▽ W.S. ELEV. -3.0'
-2.8'
-2.9'

0.0' DEPTH OF PENETRATION

LEGEND FOR BOTTOM ELEVATIONS

- APR. 2021: -0.0'
- APR. 2020: -0.0'
- FEB. 2019: -0.0'

LEGEND FOR MUDLINE CONTOURS

- APR. 2021
- APR. 2020
- FEB. 2019

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Rocky Hill, CT 06067
PH.: 860.840.2505

CONSOR

CONNECTICUT DEPARTMENT OF TRANSPORTATION

BRIDGE NO. 04671
CAROL ROAD OVER PACHAUG RIVER

GRISWOLD CONNECTICUT

ABUTMENT 1

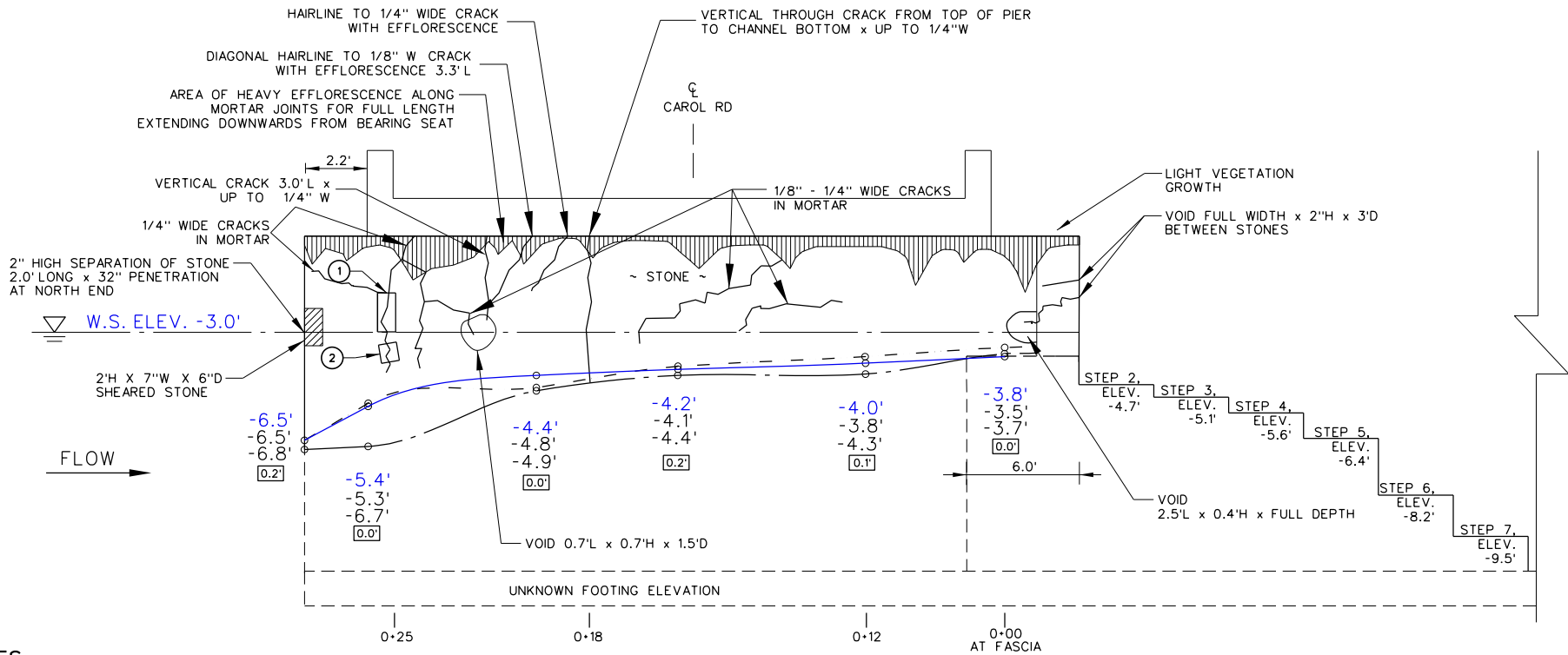
INSPECTED: SS, MB, AT, GM
REVISD BY: FH

SCALE: AS SHOWN

DATE OF INSPECTION: 04 / 14 / 21

DRAWING NO. 04671 F

UW DRAWING 3



NOTES

1. INTERMITTENT CRACKING OF MORTAR UP TO 1/4"W WAS OBSERVED FROM 1.0' ABOVE THE WATERLINE TO THE BEARING SEAT.
2. MORTAR LOSS, UP TO 95%, EXTENDING FROM WATERLINE TO THE CHANNEL BOTTOM ON THE SOUTH, EAST AND WEST FACES.
3. LOOSE AND MISSING CHINKING STONES, UP TO 30%, BELOW THE WATERLINE.
4. MISSING MORTAR, UP TO 100%, WITH UP TO 4.6' PENETRATION INTO THE STRUCTURE, FROM 1.0' ABOVE THE WATERLINE TO THE CHANNEL BOTTOM, ON THE NORTH FACE.
5. STONES ARE 6" WIDER AT N,E & W FACES STARTING 0.5' BELOW WATER SURFACE.
6. SEVERAL VOIDS HAVE BEEN PATCHED AND MASONRY HAS BEEN REPOINTED.

PIER NO. 1 (WEST) ELEVATION
N.T.S.

VOID NO.	VOID TABLE		
	ENGLISH DIMENSIONS		
	LENGTH	VERTICAL HEIGHT	PENETRATION
①	0.8'	1.0'	FD
②	0.5'	0.5'	FD

LEGEND FOR SYMBOLS

- ⊕ DATUM ELEV. 0.0' TAKEN FROM THE TOP OF WINGWALL 2A, AT ABUTMENT 2.
- ▽ W.S. ELEV. -3.0'
-2.8'
-2.9'


① DEPTH OF PENETRATION

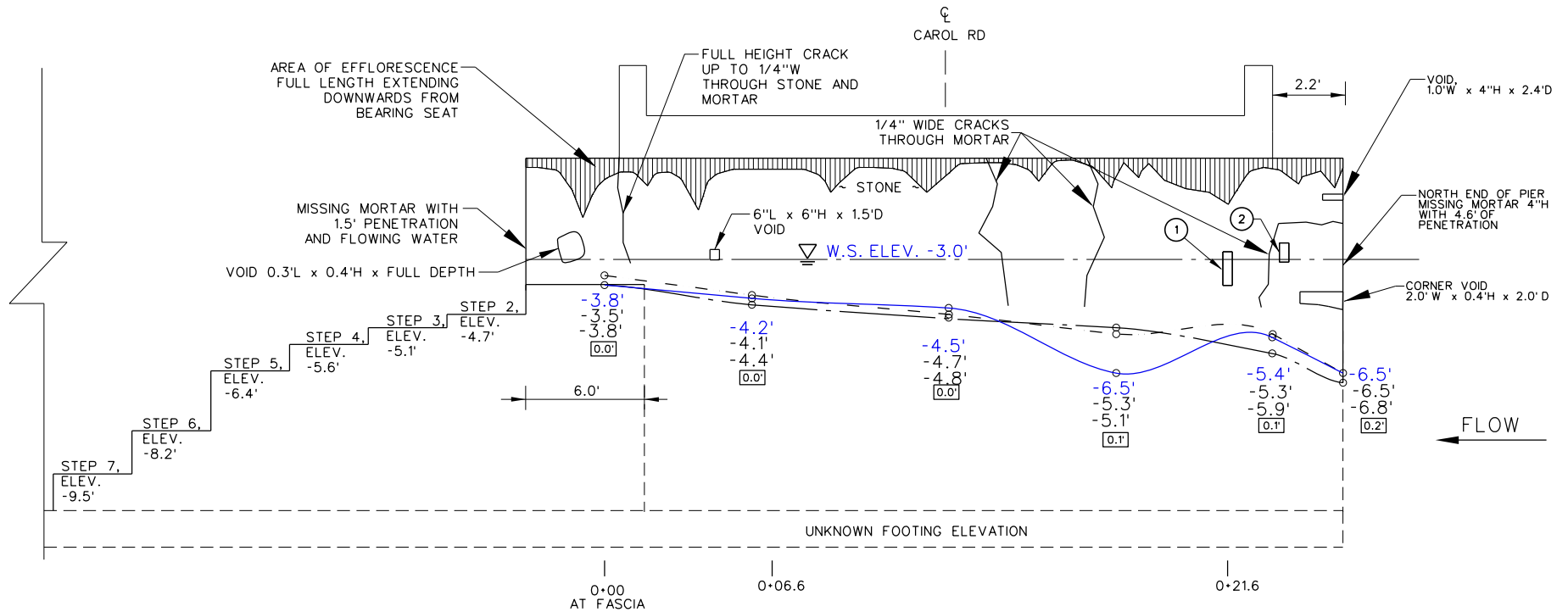
LEGEND FOR BOTTOM ELEVATIONS

APR. 2021: -0.0'
APR. 2020: -0.0'
FEB. 2019: -0.0'

LEGEND FOR MUDLINE CONTOURS

— APR. 2021
- - - APR. 2020
- - - FEB. 2019

 <p>50 Inwood Road, Suite 101 Rocky Hill, CT 06067 PH.: 860.840.2505</p>	<p>CONNECTICUT DEPARTMENT OF TRANSPORTATION</p> <p>BRIDGE NO. 04671 CAROL ROAD OVER PACHAUG RIVER</p> <p>CONNECTICUT</p>			<p>UW DRAWING 4</p>
	<p>PIER NO. 1 (WEST) ELEVATION</p>			
<p>INSPECTED: SS, MB, AT, GM REVISED BY: FH</p>	<p>SCALE: AS SHOWN</p>	<p>DATE OF INSPECTION 04 / 14 / 21</p>	<p>DRAWING NO. 04671 G</p>	



NOTES

1. INTERMITTENT CRACKING OF MORTAR WAS OBSERVED FROM 1.0' ABOVE THE WATERLINE TO THE BEARING SEAT.
2. MORTAR LOSS, UP TO 95%, EXTENDING FROM THE WATERLINE TO THE CHANNEL BOTTOM WITH LESS THAN 15% MORTAR LOSS ABOVE THE WATER LINE WITH 10" TYPICAL PENETRATION AT ALL LOCATIONS.
3. LOOSE AND MISSING CHINKING STONES, UP TO 20%, BELOW THE WATERLINE WITH 1' PENETRATION.
4. STONES UP TO 12" WIDER AT N,E & W FACES STARTING 0.5' BELOW WATER SURFACE.
5. SEVERAL VOIDS HAVE BEEN PATCHED AND MASONRY HAS BEEN REPOINTED.

PIER NO. 1 (EAST) ELEVATION
N.T.S.

VOID TABLE				
ENGLISH DIMENSIONS				
VOID NO.	LENGTH	VERTICAL HEIGHT	PENETRATION	NOTES
①	0.2'	0.8'	1.7'	
②	0.2'	0.5'	3.0'	FD

LEGEND FOR SYMBOLS

- ⊕ DATUM ELEV. 0.0' TAKEN FROM THE TOP OF WINGWALL 2A, AT ABUTMENT 2.
- ▽ W.S. ELEV. -3.0'
-2.8'
-2.9'

[0.0'] DEPTH OF PENETRATION

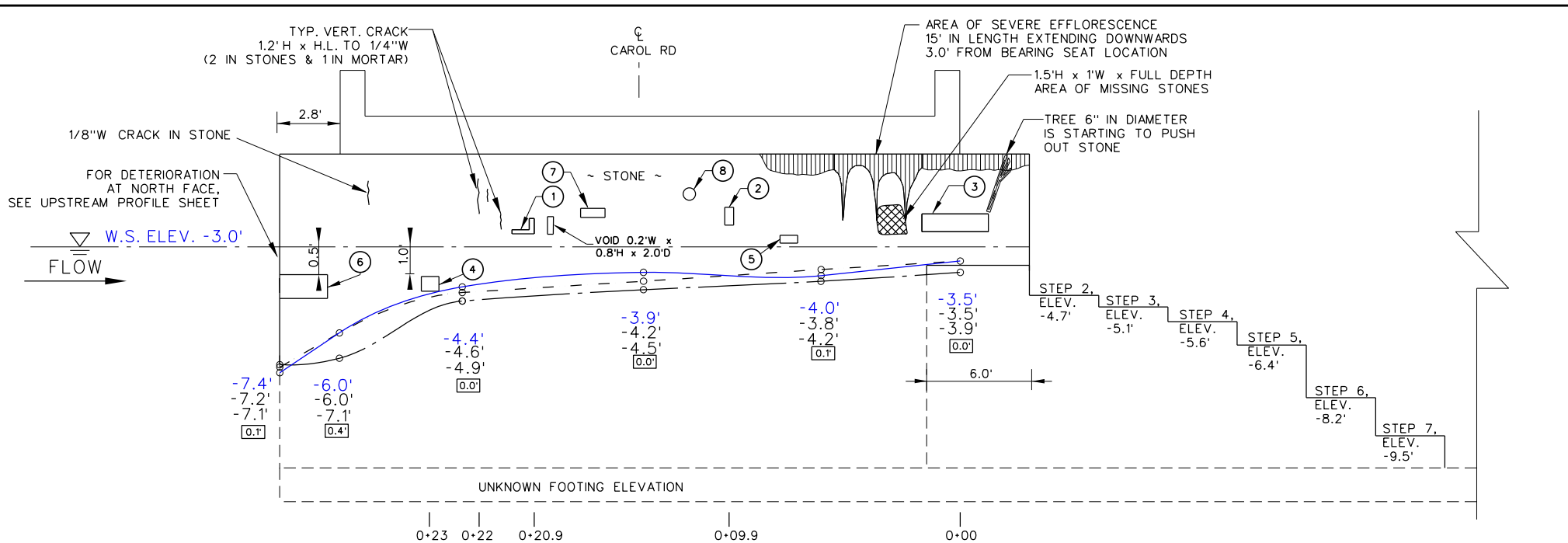
LEGEND FOR BOTTOM ELEVATIONS

APR. 2021: -0.0'
APR. 2020: -0.0'
FEB. 2019: -0.0'

LEGEND FOR MUDLINE CONTOURS

— APR. 2021
- - - APR. 2020
- - - FEB. 2019

<p>50 Inwood Road, Suite 101 Rocky Hill, CT 06067 PH.: 860.840.2505</p>	<p>CONNECTICUT DEPARTMENT OF TRANSPORTATION</p> <p>BRIDGE NO. 04671 CAROL ROAD OVER PACHAUG RIVER</p> <p>GRISWOLD CONNECTICUT</p>			<p>UW DRAWING 5</p>
	<p>PIER NO. 1 (EAST) ELEVATION</p>			
	<p>INSPECTED: SS, MB, AT, GM REVISED BY: FH</p>	<p>SCALE: AS SHOWN</p>	<p>DATE OF INSPECTION 04 / 14 / 21</p>	



PIER NO. 2 (WEST) ELEVATION
N.T.S.

NOTES

1. INTERMITTENT HAIRLINE CRACKING OF MORTAR IN JOINTS WAS OBSERVED.
2. MORTAR LOSS, UP TO 75%, EXTENDING FROM 1.0' ABOVE THE WATERLINE TO THE CHANNEL BOTTOM WITH LESS THAN 5% MORTAR LOSS ABOVE THE WATER LINE. 15" TYPICAL PENETRATION AT ALL LOCATIONS.
3. SEVERAL VOIDS HAVE BEEN PATCHED AND MASONRY HAS BEEN REPOINTED.
4. STONES ARE 6" WIDER AT N,E & W FACES STARTING 1.0' BELOW WATER SURFACE.

VOID NO.	VOID TABLE			NOTES
	ENGLISH DIMENSIONS			
	LENGTH	VERTICAL HEIGHT	PENETRATION	
①	1.3'	0.3'	3.0'	(FULL DEPTH) PROBE 1.5' BEHIND FASCIA STONE
②	0.5'	0.5'	3.0'	SURROUNDING STONE LOOSE
③	1.0'	0.5'	1.5'	
④	1.0'	0.3'	3.0'	(FULL DEPTH)
⑤	0.1'	0.1'	1.5'	
⑥	2.7'	0.7'	2.8'	
⑦	0.4'	0.3'	3.0'	(FULL DEPTH)
⑧	0.3'	0.8'	2.0'	

LEGEND FOR SYMBOLS

- ⊕ DATUM ELEV. 0.0' TAKEN FROM THE TOP OF WINGWALL 2A, AT ABUTMENT 2.
- ▽ W.S. ELEV. -3.0'
-2.8'
-2.9'

☐ DEPTH OF PENETRATION

LEGEND FOR BOTTOM ELEVATIONS

- APR. 2021: -0.0'
- APR. 2020: -0.0'
- FEB. 2019: -0.0'

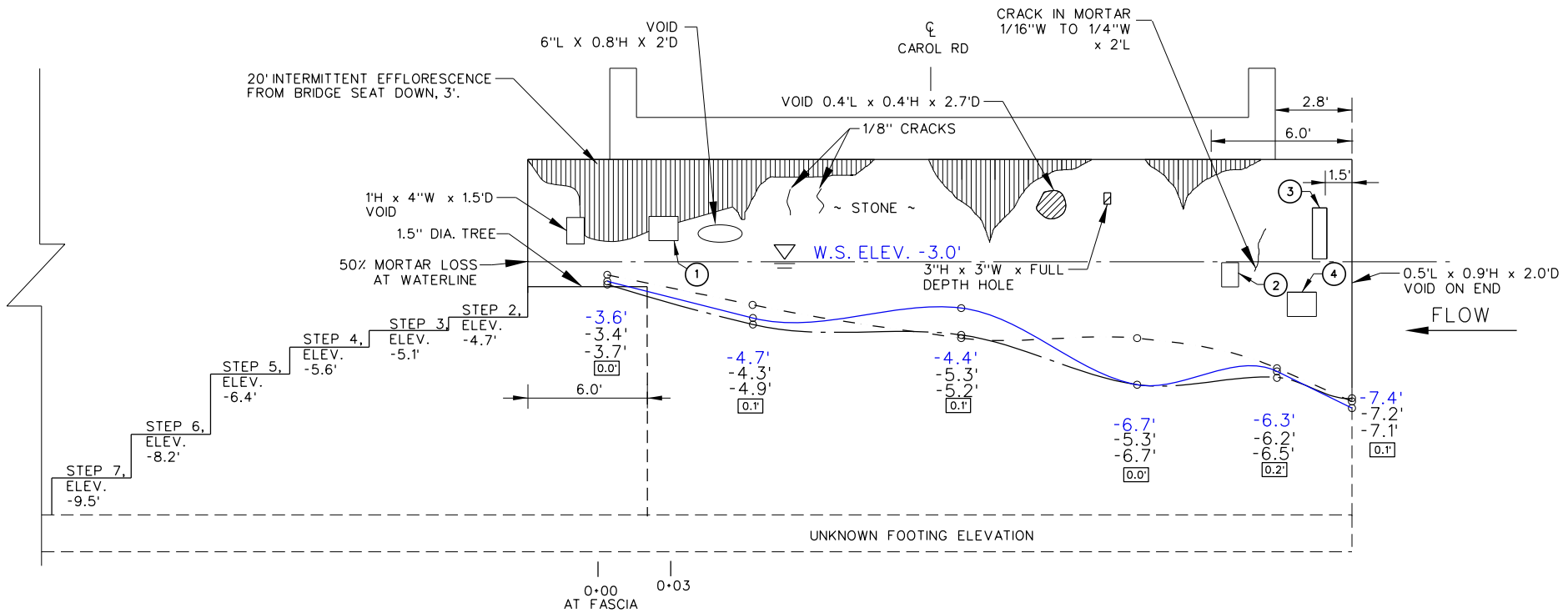
LEGEND FOR MUDLINE CONTOURS

- APR. 2021
- - - APR. 2020
- FEB. 2019


 50 Inwood Road, Suite 101
 Rocky Hill, CT 06067
 PH.: 860.840.2505
CONSOR

CONNECTICUT DEPARTMENT OF TRANSPORTATION			
BRIDGE NO. 04671			
CAROL ROAD OVER PACHAUG RIVER			
GRISWOLD			CONNECTICUT
PIER NO. 2 (WEST) ELEVATION			
INSPECTED: SS, MB, AT, GM	SCALE: AS SHOWN	DATE OF INSPECTION: 04 / 14 / 21	DRAWING NO. 04671 I
REVISED BY: FH			

UW DRAWING 6



PIER NO. 2 (EAST) ELEVATION
N.T.S.

NOTES


1. INTERMITTENT HAIRLINE UP TO 1/4" W CRACKING OF MORTAR IN JOINTS WAS OBSERVED.
2. MORTAR LOSS, UP TO 75%, EXTENDING FROM WATER SURFACE TO THE CHANNEL BOTTOM WITH LESS THAN 5% MORTAR LOSS ABOVE THE WATER LINE. 15" TYPICAL PENETRATION NOT AT ALL LOCATIONS. MISSING MORTAR, UP TO 50%, ON SOUTH FACE.
3. SEVERAL VOIDS HAVE BEEN PATCHED AND MASONRY HAS BEEN REPOINTED.
4. STONES ARE 6" WIDER AT N,E & W FACES STARTING 1.0' BELOW WATER SURFACE.

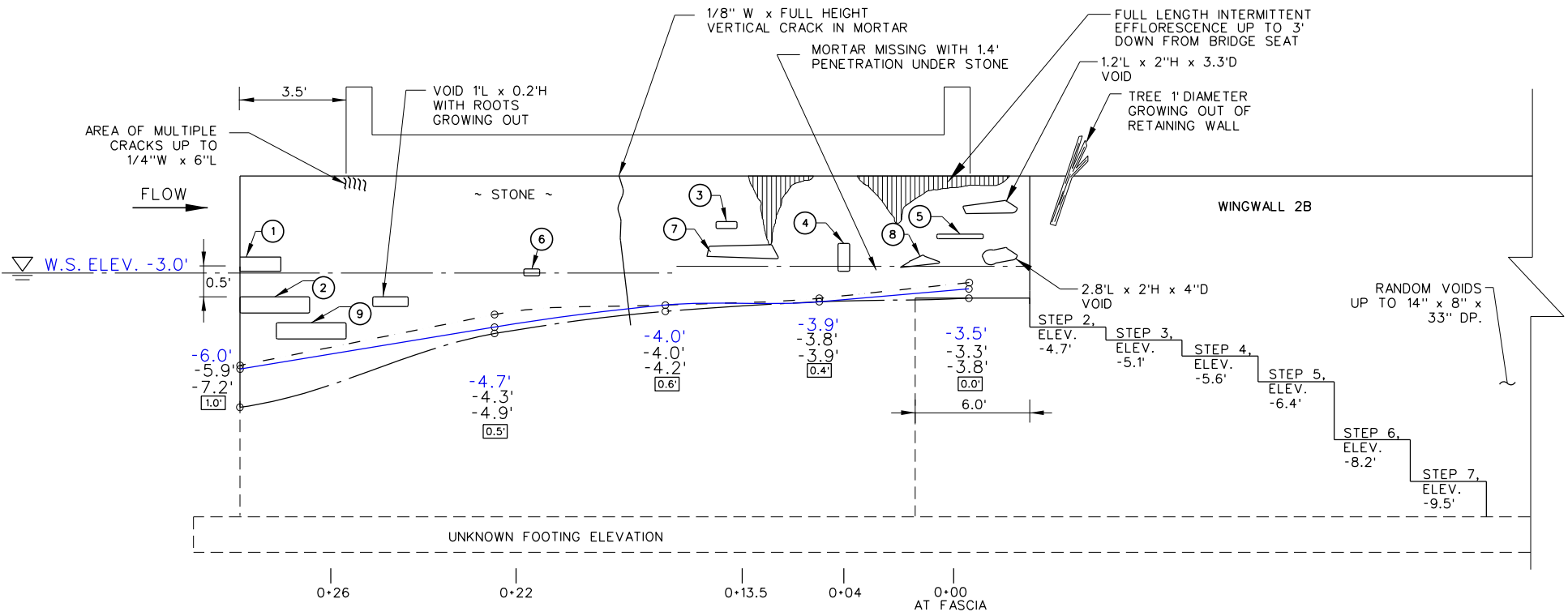
VOID NO.	VOID TABLE		
	ENGLISH DIMENSIONS		
	LENGTH	VERTICAL HEIGHT	PENETRATION
①	0.9'	1.0'	2.0'
②	0.1'	0.5'	2.0'
③	0.7'	0.8'	2.0'
④	0.8'	0.1'	2.0'

- LEGEND FOR SYMBOLS**
- ⊕ DATUM ELEV. 0.0' TAKEN FROM THE TOP OF WINGWALL 2A, AT ABUTMENT 2.
 - ▽ W.S. ELEV. -3.0'
-2.8'
-2.9'
 - ☐ DEPTH OF PENETRATION

- LEGEND FOR BOTTOM ELEVATIONS**
- APR. 2021: -0.0'
 - APR. 2020: -0.0'
 - FEB. 2019: -0.0'

- LEGEND FOR MUDLINE CONTOURS**
- APR. 2021
 - APR. 2020
 - FEB. 2019

 <p>50 Inwood Road, Suite 101 Rocky Hill, CT 06067 PH.: 860.840.2505</p> <p>CONSOR</p>	<p align="center">CONNECTICUT DEPARTMENT OF TRANSPORTATION</p> <p align="center">BRIDGE NO. 04671 CAROL ROAD OVER PACHAUG RIVER</p> <p align="center">CONNECTICUT</p>		<p>UW DRAWING 7</p>
	<p align="center">PIER NO. 2 (EAST) ELEVATION</p>		
	<p>INSPECTED: SS, MB, AT, GM REVISED BY: FH</p>	<p>SCALE: AS SHOWN</p>	



ABUTMENT 2
N.T.S.

VOID TABLE			
VOID NO.	ENGLISH DIMENSIONS		
	LENGTH	VERTICAL HEIGHT	PENETRATION
①	1.5'	0.3'	6.2'
②	1.5'	0.4'	1.5'
③	0.9'	0.3'	1.7'
④	1.5'	0.9'	1.0'
⑤	3.0'	0.5'	2.0'
⑥	0.5'	0.4'	2.9'
⑦	2.0'	0.4'	1.5'
⑧	0.5'	0.5'	2.5'
⑨	2.0'	0.7'	4.0'

(WITH LOSS OF FILL)

(WITH LOSS OF FILL, COARSE GRANULAR AND GRAVEL)

NOTES

- UP TO 20% MORTAR LOSS ABOVE WATER LINE WITH UP TO 1.0' PENETRATION. BELOW THE WATER LINE, THERE IS 100% MORTAR LOSS WITH UP TO 1.0' PENETRATION.
- SEVERAL VOIDS HAVE BEEN PATCHED AND SOME MASONRY HAS BEEN REPOINTED.

LEGEND FOR SYMBOLS

⊕ DATUM ELEV. 0.0' TAKEN FROM THE TOP OF WINGWALL 2A, AT ABUTMENT 2.

▽ W.S. ELEV. -3.0'
-2.8'
-2.9'

☐ DEPTH OF PENETRATION

LEGEND FOR BOTTOM ELEVATIONS

APR. 2021: -0.0'
APR. 2020: -0.0'
FEB. 2019: -0.0'

LEGEND FOR MUDLINE CONTOURS

— APR. 2021
- - - APR. 2020
- · - · - FEB. 2019

<p>50 Inwood Road, Suite 101 Rocky Hill, CT 06067 PH.: 860.840.2505</p>	<p align="center">CONNECTICUT DEPARTMENT OF TRANSPORTATION</p>		UW DRAWING 8
	<p align="center">BRIDGE NO. 04671 CAROL ROAD OVER PACHAUG RIVER</p>		
	<p align="center">GRISWOLD CONNECTICUT</p>		
<p align="center">ABUTMENT 2</p>			
INSPECTED: SS, MB, AT, GM REVISED BY: FH	SCALE: AS SHOWN	DATE OF INSPECTION 04 / 14 / 21	DRAWING NO. 04671 K

Form: Asset Photos

Inspection type: Routine, Underwater

Inspection Date: 4/14/2021

Inspected by: CONSOR Engineers

:Bridge No 04671

Town: GRISWOLD

Carried: CARROLL ROAD

Crossed: PACHAUG RIVER

Inventory Route: Non-NHS



Photo Number: 1

Bridge Identification Number

Photo Taken: 04/14/2021



Photo Number: 2

Upstream (North) Bridge Elevation

Photo Taken: 04/14/2021

Form: Asset Photos

Inspection type: Routine, Underwater

Inspection Date: 4/14/2021

Inspected by: CONSOR Engineers

:Bridge No 04671

Town: GRISWOLD

Carried: CARROLL ROAD

Crossed: PACHAUG RIVER

Inventory Route: Non-NHS



Photo Number: 3

Photo Taken: 04/14/2021

Downstream (South) Bridge Elevation



Photo Number: 4

Photo Taken: 04/14/2021

Bridge from the West Approach

Form: Asset Photos

Inspection type: Routine, Underwater

Inspection Date: 4/14/2021

Inspected by: CONSOR Engineers

:Bridge No 04671

Town: GRISWOLD

Carried: CARROLL ROAD

Crossed: PACHAUG RIVER

Inventory Route: Non-NHS



Photo Number: 5

Photo Taken: 04/14/2021

West Approach from the Bridge



Photo Number: 6

Photo Taken: 04/14/2021

Bridge from the East Approach

Form: Asset Photos

Inspection type: Routine, Underwater

Inspection Date: 4/14/2021

Inspected by: CONSOR Engineers

:Bridge No 04671

Town: GRISWOLD

Carried: CARROLL ROAD

Crossed: PACHAUG RIVER

Inventory Route: Non-NHS



Photo Number: 7

Photo Taken: 04/14/2021

East Approach from the Bridge



Photo Number: 8

Photo Taken: 04/14/2021

Bituminous Overlay. Looking East (Whole Bridge)

Note: Potholes / spalls, random patches with deteriorating bituminous and punky concrete throughout.

Form: Asset Photos

Inspection type: Routine, Underwater

Inspection Date: 4/14/2021

Inspected by: CONSOR Engineers

:Bridge No 04671

Town: GRISWOLD

Carried: CARROLL ROAD

Crossed: PACHAUG RIVER

Inventory Route: Non-NHS



Photo Number: 9

Photo Taken: 04/14/2021

Span 2 Overlay Near Left (North) Curb

Note: Pothole / spall.



Photo Number: 10

Photo Taken: 04/14/2021

Overlay in Span 2, Looking North

Note: Uneven overlay covered with sand with potholes / spalls, cracks.

Form: Asset Photos

Inspection type: Routine, Underwater

Inspection Date: 4/14/2021

Inspected by: CONSOR Engineers

:Bridge No 04671

Town: GRISWOLD

Carried: CARROLL ROAD

Crossed: PACHAUG RIVER

Inventory Route: Non-NHS



Photo Number: 11

Photo Taken: 04/14/2021

Span 3 Overlay Near Right (South) Curb

Note: Pothole / spall.



Photo Number: 12

Photo Taken: 04/14/2021

Left (North) Curb, Parapet and Bridge Rail

Note: Extensive spalling.



Photo Number: 13

Photo Taken: 04/14/2021

Right (South) Curb, Sidewalk, Parapet and Bridge Rail

Note: Extensive spalling.



Photo Number: 14

Photo Taken: 04/14/2021

Left (North) Curb - Typical

Note: Full length x full height spalling.

Form: Asset Photos

Inspection type: Routine, Underwater

Inspection Date: 4/14/2021

Inspected by: CONSOR Engineers

:Bridge No 04671

Town: GRISWOLD

Carried: CARROLL ROAD

Crossed: PACHAUG RIVER

Inventory Route: Non-NHS



Photo Number: 15

Photo Taken: 04/14/2021

Right (South) Curb in Span 1 - Typical

Note: Up to 6" deep (full depth) spalling extending into sidewalk.



Photo Number: 16

Photo Taken: 04/14/2021

Left (North) Parapet in Span 2

Note: Up to 5" deep spall.



Photo Number: 17

Photo Taken: 04/14/2021

Left (North) Parapet at Abutment 1

Note: 5' long x full height x 5" deep spall with exposed rebar.



Photo Number: 18

Photo Taken: 04/14/2021

Left (North) Metal Bridge Rail at Pier 1

Note: Cracked / broken weld.



Photo Number: 19

Photo Taken: 04/14/2021

Typical Drain



Photo Number: 20

Photo Taken: 04/14/2021

Southwest Approach 2 Cable Guide Rail

Note: Missing cables on timber posts.

Form: Asset Photos

Inspection type: Routine, Underwater

Inspection Date: 4/14/2021

Inspected by: CONSOR Engineers

:Bridge No 04671

Town: GRISWOLD

Carried: CARROLL ROAD

Crossed: PACHAUG RIVER

Inventory Route: Non-NHS



Photo Number: 21

Photo Taken: 04/14/2021

Northwest Approach Guide Rail



Photo Number: 22

Photo Taken: 04/14/2021

Northwest Approach Guide Rail at 50' +/- from Bridge

Note: Tear at rail splice.

Form: Asset Photos

Inspection type: Routine, Underwater

Inspection Date: 4/14/2021

Inspected by: CONSOR Engineers

:Bridge No 04671

Town: GRISWOLD

Carried: CARROLL ROAD

Crossed: PACHAUG RIVER

Inventory Route: Non-NHS



Photo Number: 23

Photo Taken: 04/14/2021

Southeast Approach

Note: No guide rail.



Photo Number: 24

Photo Taken: 04/14/2021

Northeast Approach Guide Rail

Note: Single bolted connection to bridge (as-built condition).

Form: Asset Photos

Inspection type: Routine, Underwater

Inspection Date: 4/14/2021

Inspected by: CONSOR Engineers

:Bridge No 04671

Town: GRISWOLD

Carried: CARROLL ROAD

Crossed: PACHAUG RIVER

Inventory Route: Non-NHS



Photo Number: 25

Photo Taken: 04/14/2021

West Approach Pavement

Note: Map cracking.



Photo Number: 26

Photo Taken: 04/14/2021

East Approach Pavement

Note: Potholes, map cracking, and sand accumulation.



Photo Number: 27

Photo Taken: 04/14/2021

Underside of Deck, Span 1 - Typical

Note: Longitudinal cracks with efflorescence and stalactites, spalls with exposed rebar with section loss.



Photo Number: 28

Photo Taken: 04/14/2021

Underside of Deck, Span 1

Note: Full length x up to 3/16" wide longitudinal crack and map cracking with efflorescence.



Photo Number: 29

Photo Taken: 04/14/2021

Underside of Deck, Span 1

Note: Spall with exposed rebar with section loss around drain.



Photo Number: 30

Photo Taken: 04/14/2021

Deck, Span 2 at Downstream (South) Fascia

Note: Full length spall/hollow area with punky concrete, efflo. and exposed debonded rebar with section loss.



Photo Number: 31

Photo Taken: 04/14/2021

Underside of Deck, Span 2

Note: Spall with bottom flange exposed with rust & section loss and exposed rebar with section loss.



Photo Number: 32

Photo Taken: 04/14/2021

Deck, Span 3 at Upstream (North) Fascia

Note: Spall/hollow area with exposed debonded rebar with section loss.



Photo Number: 33

Photo Taken: 04/14/2021

Underside of Deck, Span 3

Note: Spall around drain with exposed rebar with section loss.



Photo Number: 34

Photo Taken: 04/14/2021

Underside of Deck, Span 3

Note: Spall with exposed rebar with section loss.



Photo Number: 35

Photo Taken: 04/14/2021

Abutment 1 Elevation



Photo Number: 36

Photo Taken: 04/14/2021

Abutment 1 Left (North) End

Note: Waterline void.

Form: Asset Photos

Inspection type: Routine, Underwater

Inspection Date: 4/14/2021

Inspected by: CONSOR Engineers

:Bridge No 04671

Town: GRISWOLD

Carried: CARROLL ROAD

Crossed: PACHAUG RIVER

Inventory Route: Non-NHS



Photo Number: 37

Photo Taken: 04/14/2021

Abutment 1 Downstream End

Note: Efflorescence.



Photo Number: 38

Photo Taken: 04/14/2021

Abutment 2 Elevation



Photo Number: 39

Photo Taken: 04/14/2021

Abutment 2 Left (North) End

Note: Voiding with penetration at water surface.



Photo Number: 40

Photo Taken: 04/14/2021

Abutment 2 Left (North) End

Note: Void with penetration below water surface.



Photo Number: 41

Photo Taken: 04/14/2021

Abutment 2 at Right (South) End

Note: Voiding with penetration near water surface.



Photo Number: 42

Photo Taken: 04/14/2021

Wingwall 1A and Northwest Embankment

Form: Asset Photos

Inspection type: Routine, Underwater

Inspection Date: 4/14/2021

Inspected by: CONSOR Engineers

:Bridge No 04671

Town: GRISWOLD

Carried: CARROLL ROAD

Crossed: PACHAUG RIVER

Inventory Route: Non-NHS



Photo Number: 43

Photo Taken: 04/14/2021

Wingwall 1B and Southwest Retaining Wall



Photo Number: 44

Photo Taken: 04/14/2021

Wingwall 1B near Second Step

Note: Voids with penetration.

Form: Asset Photos

Inspection type: Routine, Underwater

Inspection Date: 4/14/2021

Inspected by: CONSOR Engineers

:Bridge No 04671

Town: GRISWOLD

Carried: CARROLL ROAD

Crossed: PACHAUG RIVER

Inventory Route: Non-NHS



Photo Number: 45

Photo Taken: 04/14/2021

Wingwall 2A and Northeast Retaining Wall



Photo Number: 46

Photo Taken: 04/14/2021

Wingwall 2A

Note: Voiding with penetration.

Form: Asset Photos

Inspection type: Routine, Underwater

Inspection Date: 4/14/2021

Inspected by: CONSOR Engineers

:Bridge No 04671

Town: GRISWOLD

Carried: CARROLL ROAD

Crossed: PACHAUG RIVER

Inventory Route: Non-NHS



Photo Number: 47

Photo Taken: 04/14/2021

Wingwall 2B and Southeast Retaining Wall



Photo Number: 48

Photo Taken: 04/14/2021

Pier 1 West Face



Photo Number: 49

Photo Taken: 04/14/2021

Pier 1 West at Left (North) End

Note: Full depth void.



Photo Number: 50

Photo Taken: 04/14/2021

Pier 1 Upstream (North) Nose

Note: Sheared stone with separation and penetration.

Form: Asset Photos

Inspection type: Routine, Underwater

Inspection Date: 4/14/2021

Inspected by: CONSOR Engineers

:Bridge No 04671

Town: GRISWOLD

Carried: CARROLL ROAD

Crossed: PACHAUG RIVER

Inventory Route: Non-NHS



Photo Number: 51

Pier 1 East Face

Photo Taken: 04/14/2021

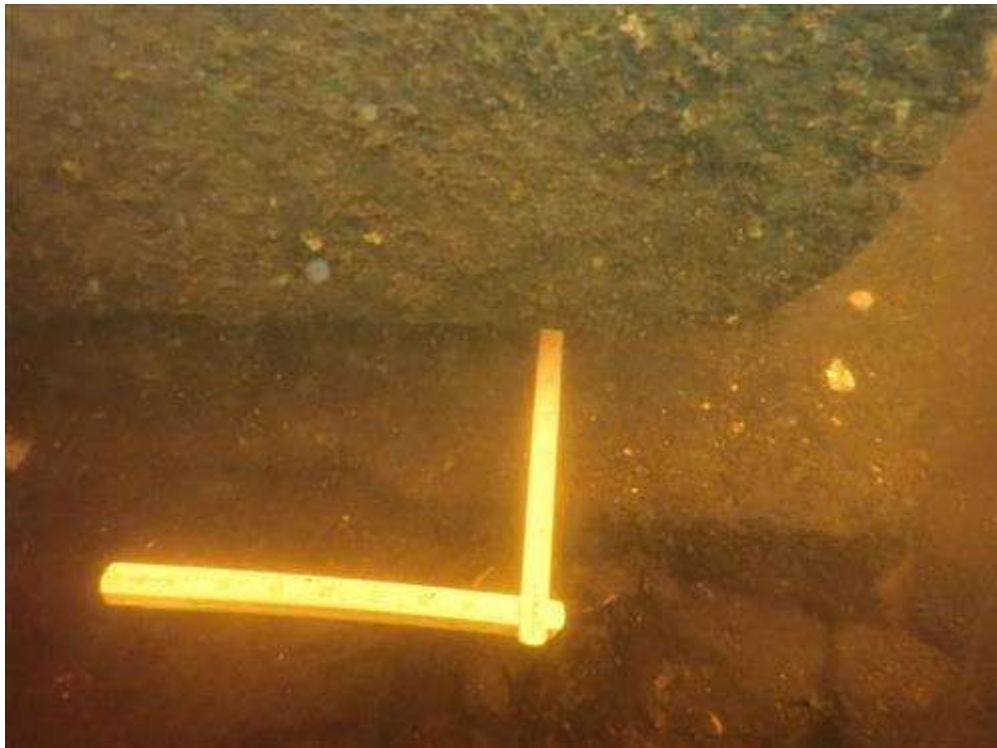


Photo Number: 52

Pier 1 East at Left (North) End Underwater

Photo Taken: 04/14/2021

Note: Voiding with penetration.



Photo Number: 53

Photo Taken: 04/14/2021

Pier 1 East at Right (South) End

Note: Void with full depth penetration.



Photo Number: 54

Photo Taken: 04/14/2021

Pier 2 West Face

Form: Asset Photos

Inspection type: Routine, Underwater

Inspection Date: 4/14/2021

Inspected by: CONSOR Engineers

:Bridge No 04671

Town: GRISWOLD

Carried: CARROLL ROAD

Crossed: PACHAUG RIVER

Inventory Route: Non-NHS

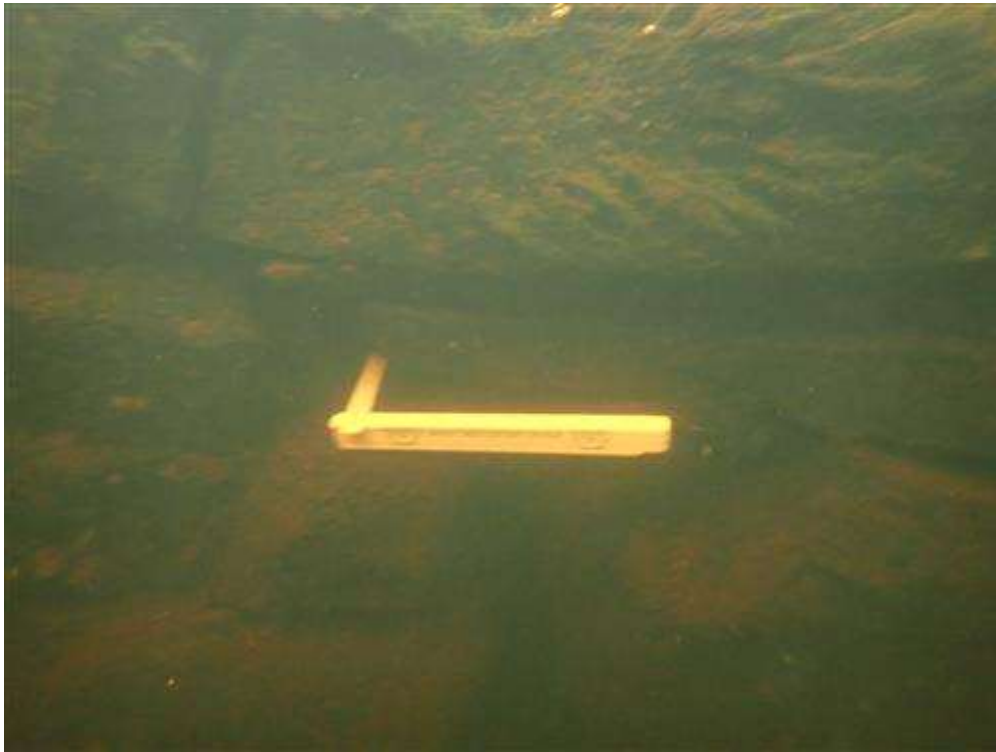


Photo Number: 55

Photo Taken: 04/14/2021

Pier 2 West Left (North) End - Underwater

Note: Mortar loss with penetration.



Photo Number: 56

Photo Taken: 04/14/2021

Pier 2 West at Right (South) End

Note: Missing stones with full depth penetration.

Form: Asset Photos

Inspection type: Routine, Underwater

Inspection Date: 4/14/2021

Inspected by: CONSOR Engineers

:Bridge No 04671

Town: GRISWOLD

Carried: CARROLL ROAD

Crossed: PACHAUG RIVER

Inventory Route: Non-NHS



Photo Number: 57

Photo Taken: 04/14/2021

Pier 2 West Downstream Vegetation Growing From Stone



Photo Number: 58

Photo Taken: 04/14/2021

Pier 2 East Face



Photo Number: 59

Photo Taken: 04/14/2021

Pier 2 East near Centerline

Note: Hole with penetration; cracks in mortar; efflorescence.



Photo Number: 60

Photo Taken: 04/14/2021

Pier 2 East at Right (South) End

Note: Void with penetration; vegetation growing.

Form: Asset Photos

Inspection type: Routine, Underwater

Inspection Date: 4/14/2021

Inspected by: CONSOR Engineers

:Bridge No 04671

Town: GRISWOLD

Carried: CARROLL ROAD

Crossed: PACHAUG RIVER

Inventory Route: Non-NHS



Photo Number: 61

Photo Taken: 04/14/2021

Channel Looking Upstream (Northeast) from the Bridge



Photo Number: 62

Photo Taken: 04/14/2021

Channel Looking Downstream (South) from the Bridge

Note: Island (area of aggradation) with trees growing.

Form: Asset Photos

Inspection type: Routine, Underwater

Inspection Date: 4/14/2021

Inspected by: CONSOR Engineers

:Bridge No 04671

Town: GRISWOLD

Carried: CARROLL ROAD

Crossed: PACHAUG RIVER

Inventory Route: Non-NHS



Photo Number: 63

Photo Taken: 04/14/2021

West Approach - Load Posting - 15 Tons



Photo Number: 64

Photo Taken: 04/14/2021

East Approach - Load Posting at Corner of Lillibridge Avenue - 15 Tons